

TO: CHAIRMAN AND MEMBERS OF THE PLANNING COMMISSION

FROM: RON WHISENAND, COMMUNITY DEVELOPMENT DIRECTOR

SUBJECT: TENTATIVE TRACT 2772 & PD 05-016 (AIRPORT ROAD BUSINESS PARK, LLC – DOUG MONDO)

DATE: OCTOBER 10, 2006

Needs: To consider applications filed by John McCarthy on behalf of Airport Road Business Park, LLC/ Doug Mondo to subdivide the 39-acre site into 36 lots ranging in size from .25-acre to 7-acres for business park development.

- Facts:
1. The project site is located on the southwest corner of Airport Road and Dry Creek Road (see attached Vicinity Map).
 2. The zoning designation for the site is AP-PD (Airport – Planned Development Overlay) and the general plan designation is BP (Business Park).
 3. In conjunction with the Tentative Tract 2772, PD 05-016 has been submitted to establish the framework for development of the Business Park. The applicant has developed Design Guidelines for the business park which will be attached to the Resolution approving the project, intended to be used as Guidelines for the development of each parcel.
 4. A Development Plan (PD) will be required to be approved by the Planning Commission for each lot. Through the PD process the Development Review Committee (DRC) and Planning Commission will have the opportunity to review the site planning, landscaping and architectural elements of the proposed project and insure that the project meets the minimum requirements of the Design Guidelines for the Business Park. Processing of PD approvals will follow action on the tentative map.
 5. The DRC has reviewed the proposed subdivision and Design Guidelines for the project at their meetings on January 9, 2006 and February 27, 2006. At the February 27th meeting the DRC, along with Staff recommended minor changes to the Guidelines. The applicant has made those changes and the revised Guidelines are attached to the PD Resolution for this report.
 6. The developable areas of all 36 lots will be located within Airport Land Use Zones 3 and 4, which allows business park development. There are portions of Lots 33, 34 & 36 that are within Zone 2, and a portion of Lot 32 is within Zone 1 & 2. The portions of the lots that do encroach into Zones 1 and 2 will be prohibited from constructing buildings in the Zone 1 and 2 areas but could utilize the areas for site amenities such as parking, landscaping and drainage. A constructive notice will be recorded over lots 32, 33, 34 & 36 indicating the limitations of development within Airport Zones 1&2.

7. A traffic study was prepared by Higgins Associates for the project (attached to the Environmental Initial Study). The Study concludes that the project will not have a significant traffic impact, because of the recommended improvements. The recommended improvements are included as mitigation measures for the project, and are summarized in the attached memo from the City Engineer.
8. Pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA) and the City's Procedures for Implementing CEQA, an Initial Study was prepared and circulated for public review and comment. Based on the information and analysis contained in the Initial Study, a determination has been made that the Project qualifies for issuance of a Mitigated Negative Declaration.

Analysis and

Conclusion:

When reviewing the subdivision proposal, Staff's main concern and focus related to the circulation of the project, specifically the Dry Creek Road and Airport Road intersection.

The initial project submittal was designed around the roads in their current location. In order to accommodate the City's future plans for a roundabout at the Airport Road / Dry Creek Road intersection, Staff requested that the project be redesigned to show Dry Creek Road oriented further to the south, in order to allow additional flexibility for designing the future roundabout. (Please see the attached Memo from the City Engineer which provides additional information related to traffic, circulation and other engineering related issues)

The proposal at this time is to create the 36-lot subdivision for business park development. PD 05-016 has been submitted in order to establish a framework for the subdivision, but does not include specific architecture for future development within the park.

The specific architectural and site planning information will be provided for on a lot by lot basis, via the submittal of a Development Plan (PD). At that time the Planning Commission and DRC will have the opportunity to review and comment on each site development and insure compliance with the General Plan, Zoning Code and Airport Land Use Plan.

Design Guidelines have been provided by the Developer as a guide for future lot owners when designing individual projects. The Guidelines have been attached as Exhibit D to the resolution to approve PD 05-016. A condition of approval has been added that will require Constructive Notice to be recorded against each lot, indicating to future property owners the requirement to apply for a PD and comply with the Development Guidelines for the business park.

The project as conditioned would meet the intent of the General Plan and Zoning Ordinance by providing the opportunity for clean attractive business to be located in the Business Park/Planned Industrial designated areas of the City.

Additionally, the subdivision of the 2.6-acre site into the smaller lots would allow smaller business owners the opportunity to own their own building which would meet the intent of the Economic Strategy by providing the opportunity to promote local industry, products and services.

Policy

Reference: General Plan Land Use & Circulation Element; General Plan Update EIR certified in 2003; Zoning Code, Economic Strategy and California Environmental Quality Act (CEQA); Airport Land Use Plan.

Fiscal

Impact: No fiscal impacts are anticipated in conjunction with these applications.

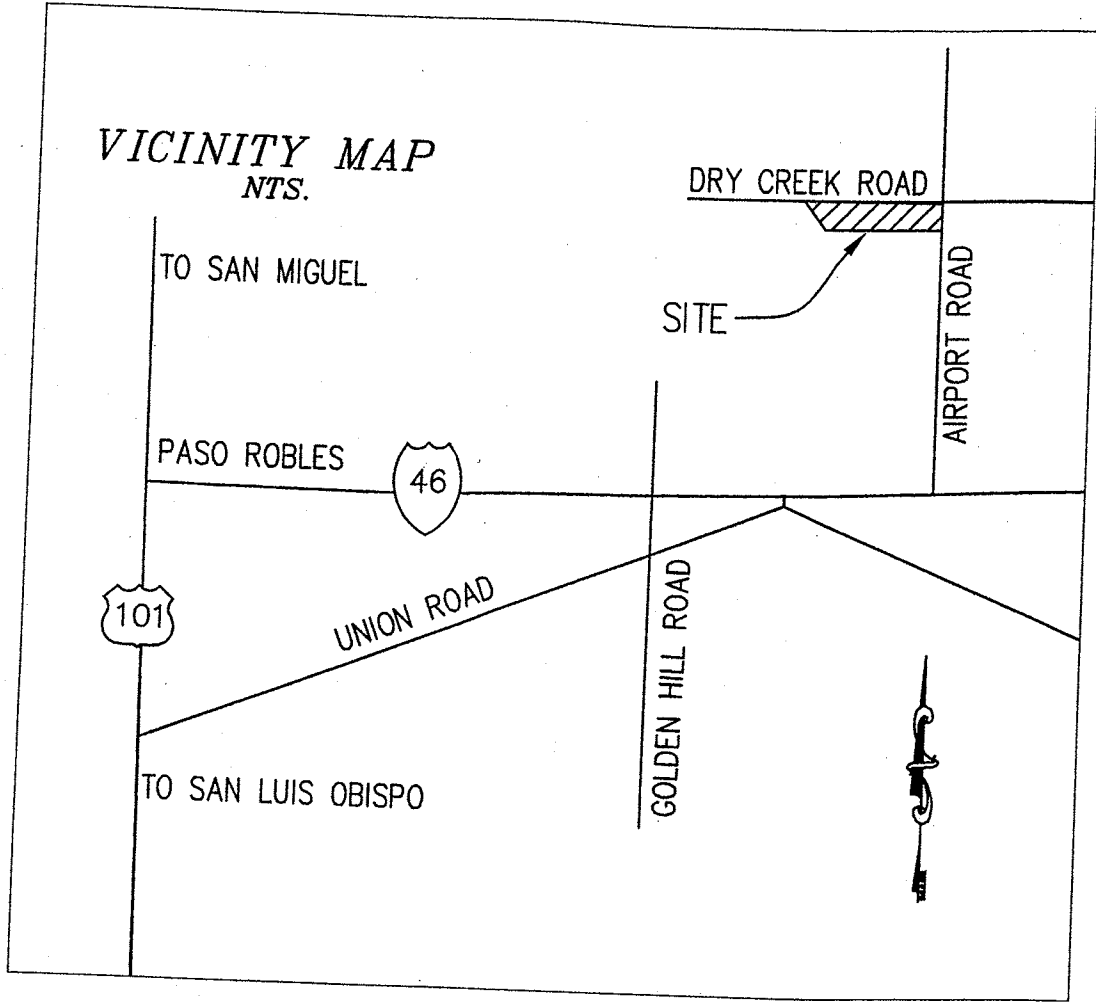
Options: After consideration of all public testimony, that the Planning Commission considers the following options:

- a. (1) Adopt a Resolution adopting a Mitigated Negative Declaration for Tentative Tract 2772 and PD 05-016;
- (2) Adopt a Resolution adopting Tentative Tract 2772, subject to site specific and standard conditions of approval;
- (3) Adopt a Resolution adopting PD 05-016 Amendment subject to site specific and standard conditions of approval;
- b. Amend, modify or reject the foregoing option.

Attachments:

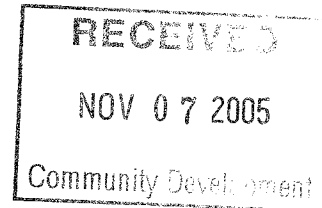
1. Vicinity Map
2. Applicant's Project Description
3. Site Plan Exhibit
4. City Engineer Memo
5. Draft Resolution approving Negative Declaration for Tract 2772 & PD 05-016
6. Draft Resolution approving Tentative Tract 2772
7. Draft Resolution approving PD 05-016
8. Newspaper and Mail Notice Affidavits

darren\PD\Mondo\PCReport



Vicinity Map
Tract 2772 & PD 05-016
(Mondo)

November 1, 2005



City of Paso Robles
Community Development Department
1000 Spring Street
Paso Robles, Ca. 93446

Subject: Project Description letter- Airport Road Business Park

Honorable Planning Commission Members:

Project Overview

We are proposing to subdivide a 39 acre parcel of land on Airport Road in order to create a Business Park. The project concept, entitled Airport Road Business Park (ARBP), provides for High Quality (Class A) R&D/ light industrial/ professional space across from the airport.

The lots will be sold to individual business owners that have expressed an interest in developing their corporate headquarters in Paso Robles. Lots will range in size from 1/2 acre to 6.5 acres. Common access will be provided by three "to be built" streets. The project will dedicate a new right of way and construct new street improvements through the property. The project will also create an architecturally pleasing main entrance. The development lots will be governed by a pre-established set of CC&R's and clearly defined Architectural Standards and development plan standards. These standards will reinforce the stated goal of developing a high quality Class A business park. Metal and wood sided buildings will not be permitted in this business park.

Community Benefits

The General Plan adopted by the City of Paso Robles in December 2003 set forth four (4) major goals. The ARBP project as outlined herein supports all four of those goals and will specifically help achieve goals two and three by strengthening the City's economic base through business retention and recruitment, and establishing Paso Robles as *the* commercial center for North County.

In order to attract and retain high end R&D/ light industrial/ professional businesses, ARBP must offer amenities that cater to and support the demands of those enterprises. As proposed, ARPB will provide the ambiance of a high-end business park, and offer key infrastructure requirements such as reliable T1 to T3 Internet connectivity. ARBP has also provided for a 2.27 acre commercial lot. This lot will be developed to support the retail needs of the business park by providing potential services such as a sandwich shop, gas station, convenience store, copy/print center, neighborhood sit-down restaurant, mail/post office center, day care center, donut/bagel shop, medical offices, dry cleaners, legal & accounting offices, and other commercial services. By addressing the inevitable retail needs of the business park within our plan, we hope to mitigate the concerns of business owners regarding potential losses in employee productivity.

Project Design


The proposed ARBP project contemplates a stately entrance structure which includes a public art component. It is envisioned that ARBP will work closely with the City and that the public art component will incorporate water features and a colorful metal sculptor. The project also plans to create landscaped mounds along Airport Road thereby providing a plush, enjoyable view from the street. The main roads within the park are planned to be tree lined and manicured to further the Class A look and feel of the park.

Business Committed

The ARBP project was created by individual business owners. Seven of the 23 lots are already spoken for by Limited Partners within ARBP. The lot commitments entail not only lot sales but actual entities ready to build company headquarters in Paso Robles. These companies include a training/education firm, a custom mail/lead generation services group, and a technology corporation. ARBP envisions working closely with City leaders and the chamber of commerce to advertise, promote and attract low impact business into this park.

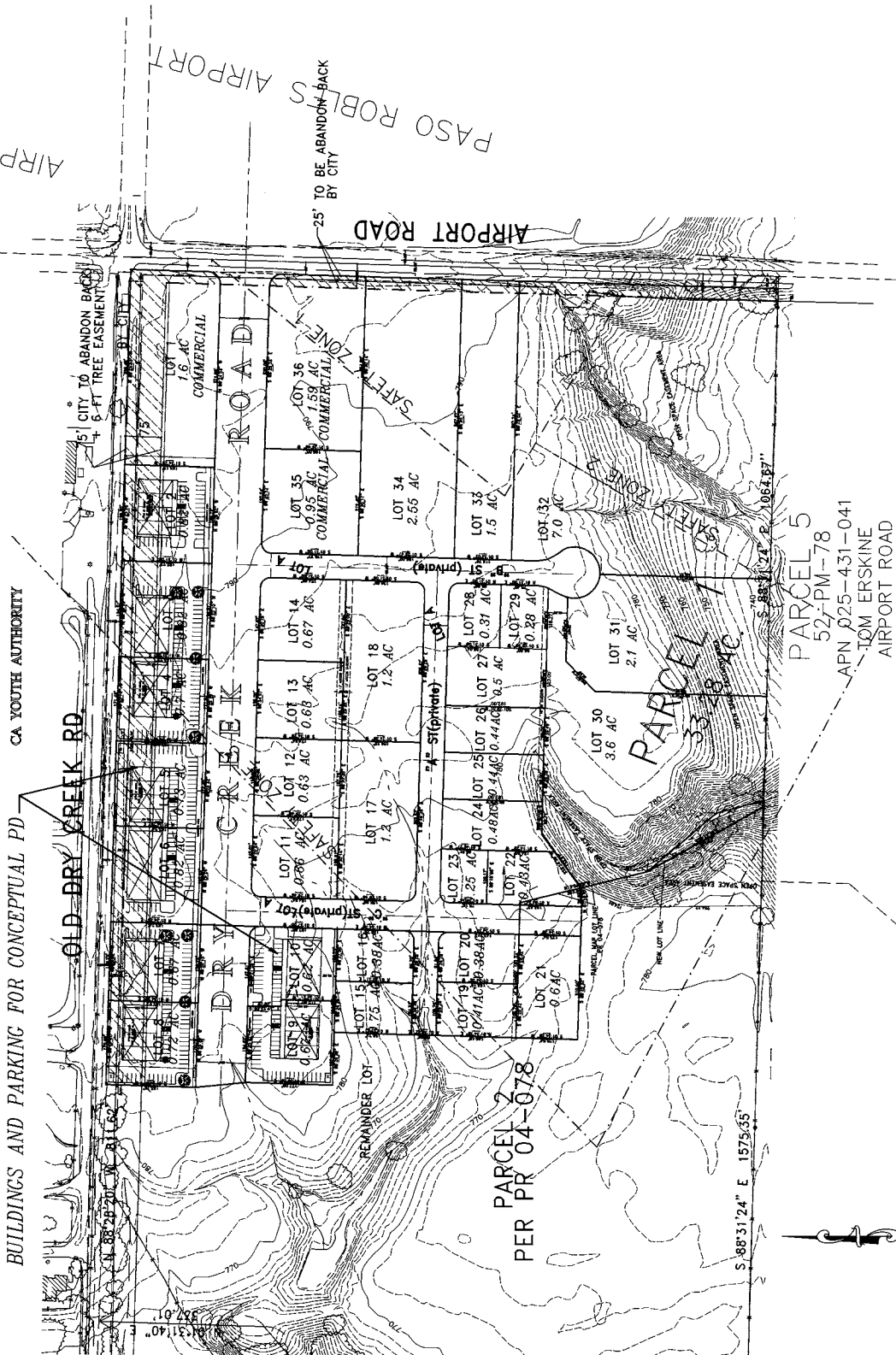
We would appreciate the Commission's approval of this request.

Sincerely,



Airport Road Business Park Management, LLC
By Doug Mondo, Managing Partner

TENTATIVE TRACT MAP 2772
 IN THE CITY OF EL PASO DE ROBLES
 CONCEPTUAL PLANNED DEVELOPMENT DETAILS - LOTS 2-10



General Notes		
No.	Revision/Note	Date

M&C
 M&C Engineering, Inc.
 10000 Elgin Road, Suite 100
 Elgin, CA 95020 (916) 437-8888

AIRPORT ROAD BUSINESS PARK
 MANAGEMENT LLC
 PLANNED DEV.
 DETAILS

Sheet	5 OF 7
Date	8-9-06
Scale	1" = 100'

Site Plan
 Tract 2772 & PD 05-016
 (Mondo)

MEMORANDUM

TO: Darren Nash
FROM: John Falkenstien
SUBJECT: Tentative Tract 2772
DATE: October 10, 2006

I have reviewed the tentative tract map and supporting documentation submitted with this application. The following are my comments.

Streets and Traffic Impacts

Dry Creek Road is classified as an Arterial Street in the circulation element of the General Plan. In accordance with the Arterial Standard A-1, the right of way for Dry Creek Road shall be 100 feet and a center landscape median will be required.

The Hogue parcels are the underlying parcels involved in Tract 2772. As a condition of the Hogue Parcel Map a 75-foot offer of dedication was made along the north boundary adjacent to a more historical 25-foot offer. There is road within the 25-foot offer of dedication that currently serves properties to the west of the State facility. Tentative Tract 2772 proposes a realignment of Dry Creek Road away from the State property.

The alignment of Dry Creek Road proposed with Tract 2772 will provide a more attractive gateway to properties to the west. In accordance with the Circulation Element, Dry Creek Road will eventually be extended to the west to connect to Golden Hill Road, providing alternative access to the Airport. Furthermore, plans for the Airport include a roundabout intersection at Dry Creek-Airport Roads, east of the existing intersection, to provide a more attractive gateway to the Airport.

To accommodate the realignment of Dry Creek Road, abandonment of the Hogue dedication along with the historical 25-foot offer will be necessary.

With the development of Tract 2772, Dry Creek Road is proposed to be constructed with two lanes and turn pockets. Two lanes will adequately serve Dry Creek Road for the life of the current General Plan.

The ultimate alignment of Airport Road adjacent to Tract 2772 has not been determined. To serve Tract 2772, pavement widening on Airport Road will be needed to provide a turn pocket for left turn access to Dry Creek Road.

Interior streets in Tract 2772 are proposed to be private. Private streets are appropriate in an industrial subdivision.

A significant portion of the Erskine property, south of Tract 2772, is bounded by the Huer Huero on the three sides and Parcel 2 of the Hogue map on the north. From a long range planning perspective, it appears that the area encompassed in the remainder parcel shown on the tentative map for Tract 2772 would provide the most reasonable

and environmentally sensitive access to the Erskine property. Remainder parcels, as defined in the Subdivision Map Act are not appropriate for offers of dedication and road improvement requirements. The intent of the remainder parcel is to set aside these issues until the remainder parcel is proposed for development. It would be appropriate to file a constructive notice on the remainder property that its development should include consideration of access to the property to the south.

The development of Tract 2772 will have impacts on Airport Road, the intersection of Airport Road and Highway 46 East, and the Highway 46 East corridor.

Sewer

Sewer is available to the project from the City's Airport trunk line located west of Tract 2772. A 12-inch sewer extension will be needed to serve the subdivision. This extension will be the beginning of a branch line that will serve all of the properties along Dry Creek Road east of Airport Road.

Water

Water is available to the property from a 16-inch water main in Airport Road.

Drainage

A storm drain is proposed for the project that will eventually discharge into Huer Huero Creek. The development of lots within the subdivision will require implementation of storm water quality devices. Erosion control measures must be installed in the subdivision to preclude silt from entering the storm drain. Additional erosion control measures must be in place downstream of the outlet of the storm drain.

A storm water infiltration basin will be required upstream of the discharge into the Huer Huero. The design of the basin will mitigate the impact of any dry season run-off from landscape irrigation in parkways and the lots themselves.

Site Specific Conditions of Approval

1. Dry Creek Road shall be constructed in accordance with the typical cross-section shown on the Tentative Map with a center median and one lane in each direction, within the boundaries of the subdivision in accordance with plans approved by the City Engineer.
2. A temporary extension of Dry Creek Road must be completed in accordance with plans approved by the City Engineer in the remainder parcel from the west boundary of Phase I to tie to the existing alignment of Dry Creek Road in order to maintain access to properties to the west. The alternate Dry Creek Road section shown on the tentative tract map will be appropriate for this section of roadway.
3. Airport Road must be improved to provide a left turn pocket for access to Dry Creek Road in accordance with plans approved by the City Engineer.
4. With each building permit, each development within Tract 2772 will provide its share of the cost of the dual left turn project at the intersection of Highways 101 and 46 East.
5. With each building permit, each development within Tract 2772 will provide its share of the cost of improvements to Airport Road from Dry Creek Road to Highway 46 E.
6. With each building permit, each development within Tract 2772 will provide its share of the cost on intersection improvements at Airport Road and Highway 46 East.
7. Constructive notice shall be filed on the Remainder Parcel that consideration of access to properties to the south must be included in any development application.
8. A 12-inch sewer main shall be extended from the Airport trunk line to the east boundary of Tract 2772 generally in accordance with the master plan of the Dry Creek Road sewer main on file in the City Public Works Department and in accordance with plans approved by the City Engineer.
9. A storm water quality and erosion control basin shall be designed and constructed to serve Tract 2772 in accordance with plans approved by the City Engineer to collect all dry season run-off from landscape irrigation in parkways and the developed lots.
10. A parkway and median planting master plan shall be provided with improvement plans for Tract 2772. All irrigation and related infrastructure shall be placed with the construction of improvements in Tract 2772. Landscape planting shall be installed with the development of each lot.

RESOLUTION NO:

A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
APPROVING A MITIGATED NEGATIVE DECLARATION FOR
PLANNED DEVELOPMENT 05-016 & TENTATIVE TRACT 2772
(AIRPORT RD. BUSINESS PARK - MONDO)
APN: 025-431-031

WHEREAS, Tract 2772 has been filed by McCarthy Engineering on behalf of Airport Road Business Park, LLC – Doug Mondo, to subdivide a 39-acre property into 36 lots ranging in size from .25-acre to 7-acre parcels, for business park development; and

WHEREAS, Tract 2772 is located on the southwest corner of Airport Road and Dry Creek Road; and

WHEREAS, in conjunction with the Tract 2772, Planned Development 05-016 has been submitted to establish development standards for the business park; and

WHEREAS, an Initial Study was prepared for this project (attached as Exhibit A) which concludes and proposes that a Mitigated Negative Declaration be approved; and

WHEREAS, Public Notice of the proposed Mitigated Negative Declaration was given as required by Section 21092 of the Public Resources Code; and

WHEREAS, a public hearing was conducted by the Planning Commission on October 10, 2006 to consider the Initial Study prepared for this application, and to accept public testimony regarding this proposed environmental determination, and

WHEREAS, the applicant has entered into a signed Mitigation Agreement with the City of Paso Robles (prior to Planning Commission action on the Negative Declaration) that establishes obligation on the part of the property owner to mitigate potential future impacts as identified within the environmental document; and

WHEREAS, based on the information contained in the Initial Study prepared for this project and testimony received as a result of the public notice, the Planning Commission finds no substantial evidence that there would be a significant impact on the environment based on the attached Mitigation Agreement and mitigation measures described in the initial study and contained in the resolution approving Tentative Tract 2772 as site specific conditions summarized below.

<u>Topic of Mitigation</u>	<u>Condition #</u>
Kit Fox /Biological	13
Traffic and Circulation	17-22

NOW, THEREFORE, BE IT RESOLVED, by the Planning Commission of the City of El Paso de Robles, based on its independent judgment, to approve a Mitigated Negative Declaration for Planned Development 05-016 and Tentative Tract 2772 in accordance with the California Environmental Quality Act.

PASSED AND ADOPTED THIS 10th day of October, 2006, by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

CHAIRMAN JOHN HAMON

ATTEST:

RON WHISENAND, PLANNING COMMISSION SECRETARY

**ENVIRONMENTAL INITIAL STUDY CHECKLIST FORM
CITY OF PASO ROBLES
PLANNING DIVISION**

- 1. PROJECT TITLE:** Planned Development 05-016 & Tentative Tract 2772
- Concurrent Entitlements:** As described above
- 2. LEAD AGENCY:** City of Paso Robles
1000 Spring Street
Paso Robles, CA 93446
- Contact:** Darren R. Nash, Associate Planner
Phone: (805) 237-3970
- 3. PROJECT LOCATION:** Southeast corner of Airport Road and Dry Creek Road, Paso Robles, California
- 4. PROJECT PROPONENT:** Airport Road Business Park Management, LLC
- Contact Person:** Doug Mondo
Phone: 238-2642
- 5. GENERAL PLAN DESIGNATION:** Business Park (BP)
- 6. ZONING:** Airport Planned Development (AP,PD)
- 7. PROJECT DESCRIPTION:** Request to subdivide the 39-acre site into 27 lots for business park development, and one remainder lot.
- 8. ENVIRONMENTAL SETTING:**
The subject property is located in the City of Paso Robles, at the southwest corner of Airport Road and Dry Creek Road. The Paso Robles Municipal Airport is located to the northeast, and the California Youth Authority property is to the north. The elevation is approximately 790 feet above sea level. The property is relatively flat, sloping to a ravine along the south end of the site. There are oak trees on the site but out of the project area.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------------	--	--------------------------------------	------------------------------------	-----------

9. RELATED ENVIRONMENTAL DOCUMENTATION:

Biological Report, prepared by Althouse and Meade, Inc., dated June 2005.

10. PERSONS PARTICIPATING IN THE PREPARATION OF THE INITIAL STUDY:

Darren Nash: Associate Planner.

11. CONTEXT OF ENVIRONMENTAL ANALYSIS FOR PROJECT:

This environmental initial study analyzes the potential impacts of the 39.15-acre site (Parcel 1 of a larger 107-acre site) that may be associated with the 36-lot business park subdivision.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” or is “Potentially Significant Unless Mitigated,” as indicated by the checklist on the following pages.

- | | | |
|---|--|--|
| <input type="checkbox"/> Land Use & Planning | <input checked="" type="checkbox"/> Transportation/Circulation | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Population & Housing | <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Utilities & Service Systems |
| <input type="checkbox"/> Geological Problems | <input type="checkbox"/> Energy & Mineral Resources | <input type="checkbox"/> Aesthetics |
| <input type="checkbox"/> Water | <input type="checkbox"/> Hazards | <input type="checkbox"/> Cultural Resources |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Noise | <input type="checkbox"/> Recreation |
| | <input type="checkbox"/> Mandatory Findings of Significance | |

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------------	--	------------------------------------	-----------

ISSUES (and Supporting Information Sources):

DETERMINATION

(To be completed by the Lead Agency)

On the basis of this initial evaluation:

I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.

I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION** will be prepared.

I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.

I find that the proposed project **MAY** have a significant effect(s) on the environment, but one or more effects (1) have been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) have been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a “potentially significant impact” or is “potentially significant unless mitigated.” An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effect(s) that remain to be addressed.

Signature

Date

Darren R. Nash

Associate Planner

Printed Name

Title

	Potentially Significant	Potentially Significant	Potentially Significant	Potentially Significant
ISSUES (and Supporting Information Sources):	Unless Mitigation Incorporated	Less Than Significant Impact		No Impact

EVALUATION OF ENVIRONMENTAL IMPACTS:

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards.
2. All answers must take account of the whole action involved. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more “Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.
4. Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVII, “Earlier Analyses,” may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). Earlier analyses are discussed in Section XVII at the end of the checklist.
6. References to information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the checklist. A source list has been provided in Section XVII. Other sources used or individuals contacted have been cited in the respective discussions.
7. The following checklist has been formatted after Appendix I of Chapter 3, Title 14, California Code of Regulations, but has been augmented to reflect the needs and requirements of the City of Paso Robles.

(Note: Standard Conditions of Approval - The City imposes standard conditions of approval on projects which are considered to be components of or modifications to the project, some of these standard conditions also result in reducing or minimizing environmental impacts to a level of insignificance. However, because they are considered part of the project, they have not been identified as mitigation measures. For the readers’ information, a list of applicable standard conditions identified in the discussions has been provided as an attachment to this document.)

SAMPLE QUESTION:

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------------	--	------------------------------------	-----------

ISSUES (and Supporting Information Sources):	<i>Potentially Significant Impact</i>	<i>Potentially Significant Unless Mitigation Incorporated</i>	<i>Less Than Significant Impact</i>	<i>No Impact</i>
--	---	---	---	------------------

Would the proposal result in or expose people to potential impacts involving:

Landslides or Mud flows? (Sources: 1, 6)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The attached source list explains that 1 is the Paso Robles General Plan and 6 is a topographical map of the area which show that the area is located in a flat area. (Note: This response probably would not require further explanation).

I. LAND USE AND PLANNING. Would the Proposal:

a) Conflict with general plan designation or zoning? (Source: 1,2)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The proposal to subdivide the 39-acre site into 36-lots for business park development is a permitted use in the AP-PD Zone, and is consistent with the Business Park (BP) General Plan designation.

b) Conflict with applicable environmental plans or policies adopted by agencies with jurisdiction over the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: There are no other environmental plans currently in place for the property by other agencies.

c) Be incompatible with existing land use in the vicinity? (Source: 1,2)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: Subdividing the site into parcels for future business park use would be consistent with subdivision within and around the airport that currently have industrial/business park uses.

The California Youth Authority is located directly to the north of the subject site. There are no restrictions for business park use in the vicinity of the Authority.

d) Affect agricultural resources or operations (e.g., impacts to soils or farmlands, or impacts from incompatible uses)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The site is currently covered with native grasses. There would not appear to be a conflict with agricultural resources. Other portions of the 107 acre site (Parcels 2 and 3) are currently planted in vineyards.

e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
---	--------------------------	--------------------------	--------------------------	-------------------------------------

Discussion: The surround designations are similar to this site, there is not an established community in this area of the City.

II. POPULATION AND HOUSING. Would the proposal:

a) Cumulatively exceed official regional or local population

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------	--	------------------------------	-----------

projections? (Source: Paso Robles General Plan.)

Discussion: There is no residential development proposed with this General Plan with the subdivision, therefore this project will not exceed population projections.

b) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)?

Discussion: The site is in the vicinity of existing roads/highways. The infrastructure in the area such as sewer and water is in the vicinity of this site and can be extended to serve the project.

c) Displace existing housing, especially affordable housing?

Discussion: N?A

III. GEOLOGIC PROBLEMS. Would the proposal result in or expose people to potential impacts involving:

a) Fault rupture?

Discussion: This portion of San Luis Obispo County (generally the Paso Robles area) is located at the far southerly end of the Salinas Valley which also extends up into Monterey County. There are two known fault zones on either side of this valley. The San Marco-Rinconada Fault system runs on the west side of the valley. The San Andreas Fault is on the east side of the valley and runs through the community of Parkfield east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the Uniform Building Code to all new development within the City. Soils reports and structural engineering in accordance with local seismic influences would be applied in conjunction with any new development proposal. Based on standardly applied conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant.

b) Seismic ground shaking?

Discussion: See the response to Section III(a). Based on that response, the potential for exposure of persons or property to seismic hazards is not considered significant.

c) Seismic ground failure, including liquefaction?

Discussion: The City's General Plan contains public safety policies that would require special attention to projects with potential for liquefaction. Also, see the response to Section III(a). Based on the above discussion, the potential for exposure of persons or property to seismic hazards, including liquefaction is not considered significant.

d) Seiche, tsunami, or volcanic hazard?

Discussion: The project site is not located in an area identified at risk for seiche, tsunami, or volcanic hazards.

e) Landslides or Mud flows?

Discussion: See discussion for III (f).

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
f) Erosion, changes in topography or unstable soil conditions from excavation, grading, or fill? Discussion: See the discussion in Section III(a). In addition to standard erosion control measures being part of a future development, all grading would be subject to standard conditions of approval ensuring that soils conditions are suitable for the proposed structures and improvements. As such, no significant impacts are anticipated.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Subsidence of the land? Discussion: See the discussion in Sections III (a) and (f) above. No significant adverse impacts are anticipated.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Expansive soils? Discussion: See the discussion in Sections III (a) and (f) above. No significant adverse impacts are anticipated.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
i) Unique geologic or physical features? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

IV. WATER. Would the proposal result in:

a) Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff? (Source: 6,7,9) Discussion: In the future, when a development plan is submitted for each lot, a standard condition of approval would be added to the project that would require the applicant to submit a complete grading and drainage plan prepared by a registered civil engineer with the improvement plans. Drainage calculations shall be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Exposure of people or property to water related hazards such as flooding? Discussion: See comment for IV.a	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Discharge into surface waters or other alteration of surface water quality (e.g. temperature, dissolved oxygen, turbidity)? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Changes in the amount of surface water in any water body? Discussion: See Sec. IV a, discussion	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Changes in currents, or the course or direction of water movement? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
groundwater recharge capability? Discussion: N/A				
g) Altered direction or rate of flow of groundwater? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Impacts to groundwater quality? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Substantial reduction in the amount of groundwater otherwise available for public water supplies? (source: 7) Discussion: It is not anticipated that the amount of ground water will be any more than typically used for a business park/light-industrial type use.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

V. AIR QUALITY. Would the proposal:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Violate any air quality standard or contribute to an existing or projected air quality violation? (Source: 9,10) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The San Luis Obispo County area is a non-attainment area for the State standards for ozone and suspended particulate matter. The SLO County Air Pollution Control District (APCD) administers a permit system to ensure that stationary sources do not collectively create emissions which would cause local and state standards to be exceeded. To aid in the assessment of project impacts subject to CEQA review, the APCD published the "CEQA Air Quality Handbook" in August, 1995. This handbook establishes screening thresholds for measuring the potential of projects to generate air quality impacts. Generally, any project that generates less than 10lbs./day of emissions would "qualify" for a Negative Declaration determination, and a project that generates between 10 and 24lbs./day of emissions would "qualify" for a Mitigated Negative Declaration.

Standard APCD conditions have been applied to this project. Conditions are listed within the Tract 2772 resolution.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Expose sensitive receptors to pollutants? (Source: 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: There would not be an exposure to sensitive receptors to pollutants with the approval of this project.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c) Alter air movement, moisture, or temperature? (Source: 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: N/A.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| d) Create objectionable odors? (Source: 10) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: N/A

VI. TRANSPORTATION/CIRCULATION. Would the

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact
--	--------------------------------------	--	--	-----------

proposal result in:

- a) Increased vehicle trips or traffic congestion? Source 13

Discussion: A Traffic Analysis Report was prepared by Higgins Associates for the project.

The City Engineer has reviewed the Traffic Report and has indicated the following comments and suggested mitigation:

Streets and Traffic Impacts:

Dry Creek Road is classified as an Arterial Street in the circulation element of the General Plan. In accordance with the Arterial Standard A-1, the right of way for Dry Creek Road shall be 100 feet and a center landscape median will be required.

The Hogue parcels are the underlying parcels involved in Tract 2772. As a condition of the Hogue Parcel Map a 75-foot offer of dedication was made along the north boundary adjacent to a more historical 25-foot offer. There is road within the 25-foot offer of dedication that currently serves properties to the west of the State facility. Tentative Tract 2772 proposes a realignment of Dry Creek Road away from the State property.

The alignment of Dry Creek Road proposed with Tract 2772 will provide a more attractive gateway to properties to the west. In accordance with the Circulation Element, Dry Creek Road will eventually be extended to the west to connect to Golden Hill Road, providing alternative access to the Airport. Furthermore, plans for the Airport include a roundabout intersection at Dry Creek-Airport Roads, east of the existing intersection, to provide a more attractive gateway to the Airport.

To accommodate the realignment of Dry Creek Road, abandonment of the Hogue dedication along with the historical 25-foot offer will be necessary.

With the development of Tract 2772, Dry Creek Road is proposed to be constructed with two lanes and turn pockets. Two lanes will adequately serve Dry Creek Road for the life of the current General Plan.

The ultimate alignment of Airport Road adjacent to Tract 2772 has not been determined. To serve Tract 2772, pavement widening on Airport Road will be needed to provide a turn pocket for left turn access to Dry Creek Road.

Interior streets in Tract 2772 are proposed to be private. Private streets are appropriate in an industrial subdivision.

A significant portion of the Erskine property, south of Tract 2772, is bounded by the Huer Huero on the three sides and Parcel 2 of the Hogue map on the north. From a long range planning perspective, it appears that the area encompassed in the remainder parcel shown on the tentative map for Tract 2772 would provide the most reasonable and environmentally sensitive access to the Erskine property. Remainder parcels, as defined in the Subdivision Map Act are not appropriate for offers of dedication and road improvement requirements. The intent of the remainder parcel is to set aside these issues until the remainder parcel is proposed for development. It would be appropriate to file a constructive notice on the remainder property that its development should include consideration of access to the property to the south.

The development of Tract 2772 will have impacts on Airport Road, the intersection of Airport Road and Highway 46 East, and the Highway 46 East corridor.

Project Mitigation Measures:

1. Dry Creek Road shall be constructed in accordance with the typical cross-section shown on the Tentative Map with a center median and one lane in each direction, within the boundaries of the subdivision in accordance with plans approved by the City Engineer.
2. A temporary extension of Dry Creek Road must be completed in accordance with plans approved by the City

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------	--	------------------------------	-----------

Engineer in the remainder parcel from the west boundary of Phase I to tie to the existing alignment of Dry Creek Road in order to maintain access to properties to the west. The alternate Dry Creek Road section shown on the tentative tract map will be appropriate for this section of roadway.

3. Airport Road must be improved to provide a left turn pocket for access to Dry Creek Road in accordance with plans approved by the City Engineer.
4. With each building permit, each development within Tract 2772 will provide its share of the cost of the dual left turn project at the intersection of Highways 101 and 46 East.
5. With each building permit, each development within Tract 2772 will provide its share of the cost of improvements to Airport Road from Dry Creek Road to Highway 46 E.
6. With each building permit, each development within Tract 2772 will provide its share of the cost on intersection improvements at Airport Road and Highway 46 East.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The proposed design does not create any unsafe design features.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c) Inadequate emergency access or inadequate access to nearby uses? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The GPA, Rezone and tentative subdivision would not create any impacts. At the time of the review of the development plan for each site, City Staff will review the project further to make sure there is not an impact related to this issue.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| d) Insufficient parking capacity on-site or off-site? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: There is no development proposed with this application. Upon review of a future development plan, City staff and the project engineer will need to insure that the proper parking numbers meet city codes.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| e) Hazards or barriers for pedestrians or bicyclists? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: N/A.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| f) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: N/A.

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| g) Rail, waterborne or air traffic impacts? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The portions of the project are within zones 1- 4 of the Airport Land Use Plan. The APLUP does allow development within these zones, within limited development within zones 1 and 2. As designed, the project would only have parking and landscaping within zones 1 and 2. As each lot develops, accordance with the APLUP will be required. Constructive notice will be required to be recorded against each parcel requiring conformance with the APLUP.

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact
--	--------------------------------------	--	--	-----------

VII. BIOLOGICAL RESOURCES. Would the proposal result in impacts to:

- a) Endangered, threatened or rare species or their habitats (including but not limited to: plants, fish, insects, animals, and birds)? Source 14, 15

Discussion:

The following Biological mitigation measures are recommended by Dan Meade in his Biological Report dated June 2005 and his Kit Fox Evaluation dated March 20, 2006, revised September 26, 2006, these reports are on file with the Community Development Department:

BR-1 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during the breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grasslands. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged. Construction activities shall observe a 300 foot buffer for raptor nests.

BR-2 Prior to issuance of grading and/or construction permits, the applicant shall submit evidence to the City of Paso Robles (see contact information below) that states that one or a combination of the following four San Joaquin kit fox mitigation measures has been implemented:

- a. Provide for the protection in perpetuity, through acquisition of fee or a conservation easement of **93.9** acres of suitable habitat in the kit fox corridor area (e.g. within the San Luis Obispo County kit fox habitat area, northwest of Highway 58), either on-site or off-site, and provide for a non-wasting endowment to provide for management and monitoring of the property in perpetuity. Lands to be conserved shall be subject to the review and approval of the California Department of Fish and Game (Department) and the City.

This mitigation alternative (a.), requires that all aspects of this program must be in place before City permit issuance or initiation of any ground disturbing activities.

- b. Purchase **93.9** credits in a Department-approved conservation bank, which would provide for the protection in perpetuity of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

At this time, there is no approved conservation bank that is operational in San Luis Obispo County. A conservation bank is expected to be operational in the near future. Purchase of credits must be completed prior to City permit issuance and initiation of any ground disturbing activities.

- c. If none of the above measures (a, b, or c) are available, the applicant may enter into a Mitigation Agreement with the Department, including depositing of funds into an escrow account (or other means of securing funds acceptable to the Department) which would ensure the protection in perpetuity of **93.9** acres of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring in perpetuity. The Department can provide a draft agreement to review; a signed Mitigation Agreement shall be submitted to the City prior to City permit issuance and initiation of any ground disturbing activities.

BR-3 Prior to issuance of grading and/or construction permits, the applicant shall provide evidence that they have retained a qualified biologist acceptable to the City Planning Division. The retained biologist shall perform the following monitoring activities:

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Potentially Significant Impact	Potentially Significant Impact	No Impact

- a. **Prior to issuance of grading and/or construction permits and within 30 days prior to initiation of site disturbance and/or construction**, the biologist shall conduct a pre-activity (i.e. pre-construction) survey for known or potential kit fox dens and submit a letter to the City reporting the date the survey was conducted, the survey protocol, survey results, and what measures were necessary (and completed), as applicable, to address any kit fox activity within the project limits.
- b. The qualified biologist shall conduct weekly site visits during site-disturbance activities (i.e. grading, diking, excavation, stock piling of dirt or gravel, etc.) that proceed longer than 14 days, for the purpose of monitoring compliance with required Mitigation Measures BR-3 through BR11. Site- disturbance activities lasting up to 14 days do not require weekly monitoring by the biologist unless observations of kit fox or their dens are made on-site or the qualified biologist recommends monitoring for some other reason (see BR-2-c3). When weekly monitoring is required, the biologist shall submit weekly monitoring reports to the City.
- c. **Prior to or during project activities**, if any observations are made of San Joaquin Kit fox, or any known or potential San Joaquin kit fox dens are discovered within the project limits, the qualified biologist shall re-assess the probability of incidental take (e.g. harm or death) to kit fox. At the time a den is discovered, the qualified biologist shall contact the U.S. Fish and Wildlife Service and the Department for guidance on possible additional kit fox protection measures to implement and whether or not a Federal and/or State incidental take permit is needed. If a potential den is encountered during construction, work shall stop until such time the U.S. Fish and Wildlife Service/Department determine it is appropriate to resume work.

If incidental take of kit fox during project activities is possible, **before project activities commence**, the applicant must consult with the U.S. Fish and Wildlife Service and the Department (see contact information below). The results of this consultation may require the applicant to obtain a Federal and/or State permit for incidental take during project activities. The applicant should be aware that the presence of kit foxes or known or potential kit fox dens at the project site could result in further delays of project activities.

In addition, the qualified biologist shall implement the following measures:

- 1. **Within 30 days prior to initiation of site disturbance and/or construction**, fenced exclusion zones shall be established around all known and potential kit fox dens. Exclusion zone fencing shall consist of either large flagged stakes connected by rope or cord, or survey laths or wooden stakes prominently flagged with survey ribbon. Each exclusion zone shall be roughly circular in configuration with a radius of the following distance measured outward from the den or burrow entrances:
 - a) Potential kit fox den: 50 feet
 - b) Known or active kit fox den: 100 feet
 - c) Kit fox pupping den: 150 feet
- 2. All foot and vehicle traffic, as well as all construction activities, including storage of supplies and equipment, shall remain outside of exclusion zones. Exclusion zones shall be maintained until all project-related disturbances have been terminated, and then shall be removed.
- 3. If kit foxes or known or potential kit fox dens are found on site, daily monitoring during ground disturbing activities shall be required by a qualified biologist.

BR-4 Prior to issuance of grading and/or construction permits, the applicant shall clearly delineate as a note on the project plans, that: *“Speed signs of 25 mph (or lower) shall be posted for all construction traffic to minimize the probability of road mortality of the San Joaquin kit fox”*. Speed limit signs shall be installed on the project site **within**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Mitigation Incorporated	Potentially Significant	Less Than Significant Impact	No Impact

30 days prior to initiation of site disturbance and/or construction,

In addition, **prior to permit issuance and initiation of any ground disturbing activities**, conditions BR-3 through BR-11 of the Developer's Statement/Conditions of Approval shall be clearly delineated on project plans.

BR-5 During the site disturbance and/or construction phase, grading and construction activities after dusk shall be prohibited unless coordinated through the City, during which additional kit fox mitigation measures may be required.

BR-6 Prior to issuance of grading and/or construction permit and within 30 days prior to initiation of site disturbance and/or construction, all personnel associated with the project shall attend a worker education training program, conducted by a qualified biologist, to avoid or reduce impacts on sensitive biological resources (i.e. San Joaquin kit fox). At a minimum, as the program relates to the kit fox, the training shall include the kit fox's life history, all mitigation measures specified by the City, as well as any related biological report(s) prepared for the project. The applicant shall notify the City shortly prior to this meeting. A kit fox fact sheet shall also be developed prior to the training program, and distributed at the training program to all contractors, employers and other personnel involved with the construction of the project.

BR-7 During the site-disturbance and/or construction phase, to prevent entrapment of the San Joaquin kit fox, all excavation, steep-walled holes or trenches in excess of two feet in depth shall be covered at the close of each working day by plywood or similar materials, or provided with one or more escape ramps constructed of earth fill or wooden planks. Trenches shall also be inspected for entrapped kit fox each morning prior to onset of field activities and immediately prior to covering with plywood at the end of each working day. Before such holes or trenches are filled, they shall be thoroughly inspected for entrapped kit fox. Any kit fox so discovered shall be allowed to escape before field activities resume, or removed from the trench or hole by a qualified biologist and allowed to escape unimpeded.

BR-8 During the site-disturbance and/or construction phase, any pipes, culverts, or similar structures with a diameter of four inches or greater, stored overnight at the project site shall be thoroughly inspected for trapped San Joaquin kit foxes before the subject pipe is subsequently buried, capped, or otherwise used or moved in any way. If during the construction phase a kit fox is discovered inside a pipe, that section of pipe will not be moved, or if necessary, be moved only once to remove it from the path of activity, until the kit fox has escaped.

BR-9 During the site-disturbance and/or construction phase, all food-related trash items such as wrappers, cans, bottles, and food scraps generated shall be disposed of in closed containers only and regularly removed from the site. Food items may attract San Joaquin kit foxes onto the project site, consequently exposing such animals to increased risk of injury or mortality. No deliberate feeding of wildlife shall be allowed.

BR-10 Prior to, during and after the site-disturbance and/or construction phase, use of pesticides or herbicides shall be in compliance with all local, state and federal regulations. This is necessary to minimize the probability of primary or secondary poisoning of endangered species utilizing adjacent habitats, and the depletion of prey upon which San Joaquin kit foxes depend.

BR-11 During the site-disturbance and/or construction phase, any contractor or employee that inadvertently kills or injures a San Joaquin kit fox or who finds any such animal either dead, injured, or entrapped shall be required to report the incident immediately to the applicant and City. In the event that any observations are made of injured or dead kit fox, the applicant shall immediately notify the U.S. Fish and Wildlife Service and the Department by telephone (see contact information below). In addition, formal notification shall be provided in writing within three working days of the finding of any such animal(s). Notification shall include the date, time, location and circumstances of the incident. Any threatened or endangered species found dead or injured shall be turned over immediately to the Department for care, analysis, or disposition.

BR-12 Prior to final inspection, or occupancy, whichever comes first, should any long internal or perimeter fencing be proposed or installed, the applicant shall do the following to provide for kit fox passage:

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact

- a. If a wire strand/pole design is used, the lowest strand shall be no closer to the ground than 12".
- b. If a more solid wire mesh fence is used, 8" x 12" openings near the ground shall be provided every 100 yards.

Upon fence installation, the applicant shall notify the City to verify proper installation. Any fencing constructed after issuance of a final permit shall follow the above guidelines.

BR-13 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grass lands. If nesting birds are located, no construction activities shall occur within 100 feet of nest until chicks are fledged. Construction activities shall observe a 300 foot buffer for raptor nests.

Contact Information

California Department of Fish and Game
 Central Coast Region
 P.O. Box 47
 Yountville, CA 94599
 (805) 528-8670

U.S. Fish and Wildlife Service
 Ventura Field Office
 2493 Portola Road, Suite B
 Ventura, CA 93003
 (805) 644-1766

City of Paso Robles
 Planning Department
 Darren Nash
 1000 Spring Street
 Paso Robles, CA 93446
 (805) 237-3970

BR-13 A pre-construction survey shall be conducted within thirty days of beginning work on the site to identify if badgers are using the site. The results of the survey shall be sent to the project manager, CDFG, and the City of Paso Robles.

If the pre-construction survey finds potential badger dens, they shall be inspected to determine whether they are occupied. The survey shall cover the entire property, and shall examine both old and new dens. If potential badger dens are too long to completely inspect from the entrance, a fiber optic scope shall be used to examine the den to the end. Inactive dens may be excavated by hand with a shovel to prevent re-use of dens during construction. If badgers are found in dens on the property between February and July, nursing young may be present. To avoid disturbance and the possibility of direct take of adults and nursing young, and to prevent badgers from becoming trapped in burrows during construction activity, no grading shall occur within 100 feet of active badger dens between February and July. Between July 1st and February 1st all potential badger dens shall be inspected to determine if badgers are present. During the winter badgers do not truly hibernate, but are inactive and asleep in their dens for several days at a time. Because they can be torpid during the winter, they are vulnerable to disturbances that may collapse their dens before they rouse and emerge. Therefore, surveys shall be conducted for badger dens throughout the year. If badger dens are found on the property during the pre-construction survey, the CDFG wildlife biologist for the area shall be contacted to review current allowable management practices.

BR-14 Prior to removal of any trees over 20 inches dbh, a survey shall be conducted by a qualified biologist to determine if any of the trees proposed for removal harbor sensitive bat species or maternal bat colonies. Maternal bat colonies may not be disturbed.

BR-15 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant	Less Than Significant	No Impact
		Unless Mitigation Incorporated	Impact	

bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during the breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grasslands. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged.

BR-16 Prior to commencement of project grading, protocol level surveys shall be conducted by a qualified biologist to determine if any listed species of fairy shrimp occur in ephemeral pools on the property. If vernal pool fairy shrimp are discovered, consultation with the US Fish and Wildlife Service must occur.

BR-17 Prior to commencement of project grading, a survey shall be conducted by a qualified biologist to determine if spadefoot toads are breeding in ephemeral pools on the property. Surveys shall be conducted from January to April, as conditions permit. If Western spadefoot toad are discovered, consultation with CDFG shall occur.

BR-18 Minimize impacts to rare species habitat. If less than 10 percent of any one plant population is impacted, no effect on the viability of the population is expected.

BR-19 If 10 percent or more of any one population is impacted, replace impacted rare species habitat in kind elsewhere on the property. Prepare a mitigation and monitoring plan for rare species habitat replacement on site.

- Obispo Indian paintbrush is an annual wildflower, the seeds of which can be collected in early May and sowed in proposed open space areas.

b) Locally designated species (e.g., heritage trees)?

Discussion: There are multiple oak trees located on this site. The future development plans for the site will be required to design around the trees and preserve them.

c) Locally designated natural communities (e.g., oak forest, coastal habitat, etc.)?

Discussion: There are oak tree groves located on the creek banks in the northern area of the site. These slopes along with the trees will be preserved. The development will be proposed to take place on the flatter areas away from the slope banks.

d) Wetland habitat (e.g., marsh, riparian and vernal pool)?

Discussion: N/A

e) Wildlife dispersal or migration corridors?

Discussion: N/A

VIII. ENERGY AND MINERAL RESOURCES. Would the proposal:

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Conflict with adopted energy conservation plans? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Use non-renewable resource in a wasteful and inefficient manner? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in the loss of availability of a known mineral resource that would be of future value to the region and the residents of the State? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

IX. HAZARDS. Would the proposal involve:

a) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Possible interference with an emergency response plan or emergency evacuation plan? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) The creation of any health hazard or potential hazards? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Increased fire hazard in areas with flammable brush, grass, or trees? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

X. NOISE. Would the proposal result in:

a) Increases in existing noise levels? Discussion: There is no construction associated with this application, with the future development plan, additional environmental review would take place.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Exposure of people to severe noise levels? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

PUBLIC SERVICES. Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
a) Fire protection? Discussion: Upon the development of the site, standard conditions will be added by the Fire Marshall addressing fire hydrants, sprinklers and access.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Police Protection? Discussion: During the development plan process in the future, the police department would have the opportunity to review the project and make comments.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Schools? Discussion: The project is in the vicinity of schools. Both an elementary school and the high school are within a mile away from the site. Upon the review of a development plan, for the site the school district will have the opportunity to comment on the project.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Maintenance of public facilities, including roads? Discussion: With the development of the site, whether R2 or R4 zoned, the developer would be required install the curb, gutter and sidewalk improvements along the property frontage. These improvements would have to be constructed per City Standards, and would eventually be accepted and cared for by the City.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Other governmental services? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

XII. UTILITIES AND SERVICE SYSTEMS. Would the proposal result in a need for new systems or supplies, or substantial alterations to the following utilities:

a) Power or natural gas? Discussion: Southern California Gas Company provides service to the Paso Robles area. The project is not anticipated to interfere with gas services or create an unmet demand.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Communication systems? Discussion: The Pacific Bell Company provides service to the Paso Robles and County areas. The project is not anticipated to interfere with phone/communication services.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Local or regional water treatment or distribution facilities? Discussion: N/A	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Sewer or septic tanks? (Source: 7) Discussion: The project will be required to hook up to City sewer and water.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Storm water drainage? (Source: 6)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
--	--------------------------------	--	------------------------------	-----------

Discussion: A standard condition of approval will be added to the project at the time of development that would require the applicant to submit a complete grading and drainage plan prepared by a registered civil engineer with the improvement plans. Drainage calculations will need to be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer

- f) Solid waste disposal?

Discussion: N/A

- g) Local or regional water supplies? (source: 3)

Discussion: There is no development associated with this general plan amendment and rezone, environmental impacts associated with the physical development of the site would be determined with the development plan process for a specific project.

XIII.AESTHETICS. Would the proposal:

- a) Affect a scenic vista or scenic highway? (Source: 1,9)

Discussion: There is no development proposed with this application. At the time the development plan goes through the planning process, high architectural and grading standards will be anticipated for this site.

- b) Have a demonstrable negative aesthetic effect? (Source: 1,9)

Discussion: There is no development proposed with this application. At the time the development plan goes through the planning process, high architectural and grading standards will be anticipated for this site.

- c) Create light or glare? (Source: 1,9)

Discussion: At the time of development, light shielding will be required.

XIV.CULTURAL RESOURCES. Would the proposal:

- a) Disturb paleontological resources?

Discussion: N/A

- b) Disturb archaeological resources? Source 16

Discussion: The Paso Robles area has been classified as territory occupied by the Migueleno Salinan and the Obispeno Chumash Native California populations. Past community populations have been evidenced at several sites within the Paso Robles area and unincorporated portions of the surrounding County.

A Cultural Survey was prepared by C.A. Singer & Associates on April 8, 2005 where it was concluded that “no buildings or structures more than 45 years old exist within the surveyed area and both prehistoric and early historic resources are absent. The property is not associated with any important historic event or person and has no unique characteristics. Although Pleistocene age fossils are found in the Paso Robles Formation deposits (mostly marine mammals), cultural materials have never been recovered.”

“.....future development of these 39.1 acres will not have an impact any known or suspected resources and no further investigations are recommended.”

- c) Affect historical resources?

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
Discussion: See XIV b.				
d) Have the potential to cause a physical change which would affect unique ethnic cultural values?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A.				
e) Restrict existing religious or sacred uses within the potential impact area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A				

XV.RECREATION. Would the proposal:

a) Increase the demand for neighborhood or regional parks or other recreational facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: When a development plan is studied for the site, outdoor open space will need to be provided to the City's Multifamily Code.				
b) Affect existing recreational opportunities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion N/A.				

XVI.MANDATORY FINDINGS OF SIGNIFICANCE.

a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A				
b) Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A				
c) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A				
d) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Discussion: N/A				

EARLIER ANALYSIS AND BACKGROUND MATERIALS.

Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D).

Earlier Documents Prepared and Utilized in this Analysis and Background / Explanatory Materials

<u>Reference #</u>	<u>Document Title</u>	<u>Available for Review at:</u>
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street Paso Robles, CA 93446
2	City of Paso Robles Zoning Code	Same as above
3	City of Paso Robles Environmental Impact Report for General Plan Update	Same as above
4	1977 Airport Land Use Plan	Same as above
5	City of Paso Robles Municipal Code	Same as above
6	City of Paso Robles Water Master Plan	Same as above
7	City of Paso Robles Sewer Master Plan	Same as above
8	City of Paso Robles Housing Element	Same as above
9	City of Paso Robles Standard Conditions of Approval for New Development	Same as above
10	San Luis Obispo County Air Pollution Control District Guidelines for Impact Thresholds	APCD 3433 Roberto Court San Luis Obispo, CA 93401
11	San Luis Obispo County – Land Use Element	San Luis Obispo County Department of Planning County Government Center San Luis Obispo, CA 93408
12	USDA, Soils Conservation Service, Soil Survey of San Luis Obispo County, Paso Robles Area, 1983	Soil Conservation Offices Paso Robles, Ca 93446
13	Higgins Traffic Analysis	On file
14	Althouse & Meade Kit Fox Survey	On File
15	Biological Report by Althouse & Meade. June 2005	On File
16	Cultural Survey by C.A. Singer & Assoc. dated April 2005	On File

Summary of Mitigation Measures

Description of Impact

Traffic
Biological/Kit Fox

Mitigation Measure

See Tract Resolution for list of Mitigation
See Tract Resolution for list of Mitigation

RESOLUTION NO. _____

A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
TO GRANT TENTATIVE MAP APPROVAL FOR TENTATIVE TRACT 2772
(AIRPORT RD. BUSINESS PARK - MONDO)
APN: 025-431-031

WHEREAS, Tract 2772 has been filed by McCarthy Engineering on behalf of Airport Road Business Park, LLC – Doug Mondo, to subdivide a 39-acre property into 36 lots ranging in size from .25-acre to 7-acre parcels, for business park development; and

WHEREAS, Tract 2772 is located on the southwest corner of Airport Road and Dry Creek Road; and

WHEREAS, in conjunction with the Tract 2772, Planned Development 05-016 has been submitted to establish development standards for the business park; and

WHEREAS, an Initial Study was prepared for this project in accordance with the California Environmental Quality Act (CEQA) and a Mitigated Negative Declaration was approved by the Planning Commission on October 10, 2006, and

WHEREAS, a public hearing was conducted by the Planning Commission on October 10, 2006 to consider facts as presented in the staff report prepared for the tentative tract map, and to accept public testimony regarding the application, and

WHEREAS, based upon the facts and analysis presented in the staff report, public testimony received and subject to the conditions listed below, the Planning Commission makes the following findings as required by Government Code Sections 66474 and 65457:

1. The proposed tentative tract map is consistent with the adopted General Plan and Zoning Code for the City of El Paso de Robles by providing the opportunity for clean attractive business to be located in the Business Park/Planned Industrial designated areas of the City;
2. The design of lots, streets, open space, drainage, sewers, water and other improvements is consistent with the General Plan and Zoning Ordinance;
3. The site is physically suitable for the type of development proposed;
4. The site is physically suitable for the proposed density of development;
5. The design of the land division is not likely to cause substantial environmental damage or substantially and unavoidably injure fish or wildlife or their habitat;
6. The design of the land division and types of improvements proposed are not likely to cause serious public health problems;

7. The design of the land division and the type of improvements proposed will not conflict with easements acquired by the public at large, for access through or use of, property within the proposed subdivision;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of El Paso de Robles, does hereby grant tentative map approval for Tentative Tract 2772 subject to the following conditions of approval:

STANDARD CONDITIONS OF APPROVAL:

1. The applicant/developer shall comply with those standard conditions which are indicated as applicable in "Exhibit A" to this resolution.
2. The project shall comply with all conditions of approval in the resolution granting approval to Planned Development 05-016 and its exhibits. In the event that either the tract or development plan is not approved, the approval of one plan does not automatically grant approval of the other.

SITE SPECIFIC CONDITIONS OF APPROVAL:

NOTE: In the event of conflict or duplication between standard and site specific conditions, the site specific condition shall supersede the standard condition.

COMMUNITY DEVELOPMENT SITE SPECIFIC CONDITIONS:

NOTE: In the event of conflict or duplication between standard and site specific conditions, the site specific condition shall supersede the standard condition.

3. The project shall be constructed so as to substantially conform with the following listed exhibits and conditions established by this resolution:

<u>EXHIBIT</u>	<u>DESCRIPTION</u>
A	Standard Conditions
B	Tentative Tract Map
C	Preliminary Grading & Drainage Plan
D	Utility Layout Plan
E	Airport Overlay Zones

4. This Tentative Tract Map 2772 coincides with Planned Development 05-016 and authorizes the subdivision of a 39-acre parcel into a 36-lot Business Park, where the parcels would range in size from .25-acre to 6-acres.

5. The Final Subdivision Map shall be in substantial compliance with the tentative subdivision map and preliminary grading plan (Exhibits B & C, reductions attached; full size copies are on file in the Community Development Department) and as amended by site specific and standard conditions contained in this resolution.
6. Prior to the approval of the final map, in conjunction with the street improvement plans, the street tree plan shall be reviewed and accepted by the Public Works Department. All necessary irrigation shall be shown on the plan.
7. Prior to or in conjunction with the recordation of the final map, a constructive notice shall be recorded on each lot, to alert future owners that the development of each lot will need to go through the Planned Development (PD) process, where the project will need to go through a public hearing process to the Planning Commission.
8. In the event that buried or otherwise hidden resources are discovered during construction work in the area of the find, work should be temporarily suspended and the City of Paso Robles should be contacted immediately, and appropriate mitigations measures shall be developed by qualified archeologist or historian if necessary, at the developers expense.

Air Quality Conditions

9. The project shall be conditioned to comply with all applicable District regulations pertaining to the control of fugitive dust (PM-10) as contained in section 6.4 of the Air Quality Handbook. All site grading and demolition plans noted shall list the following regulations:
 - a. Reduce the amount of the disturbed area where possible.
 - b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (nonpotable) water should be used whenever possible.
 - c. All dirt stockpile areas should be sprayed daily as needed.
 - d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities.
 - e. Exposed ground areas that are to be reworked at dates greater than one month after initial grading should be sown with a fast-germinating native grass seed and watered until vegetation is established.
 - f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD.

- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.
- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114.
- j. Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site.
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water should be used where feasible

10. Standard Measures:

- a. Provide on-site bicycle parking. One bicycle parking place for every 10 car parking spaces is considered appropriate.
- b. Provide preferential carpool and vanpool parking spaces.
- c. Increase the building energy efficiency rating by 10-percent above what is required by Title 24 requirements. This can be accomplished in a number of ways (increasing attic, wall, or floor insulation, installing double pane windows, using efficient interior lighting, ect.).

11. Discretionary Measures (Site design mitigation for this commercial project):

- a. Implement on-site circulation design elements in parking lots to reduce vehicle queuing and improve the pedestrian environment with designated walkways.

12. Energy Efficiency Elements: Increasing the energy efficiency for these projects will decrease the demand on electrical supply, thus reducing emissions at the power plant source. Include the following Energy Efficiency measures as feasible:

- a. use built-in-energy efficient appliances, where applicable
- b. use double-paned windows
- c. use low energy efficient parking lot and street lights (e.g. sodium).
- d. install door sweeps or weather stripping if more energy efficient doors and windows are not available.

Biological Conditions

13. The following Biological mitigation measures shall be completed as indicated by Dan Meade in his Biological Rport dated June 2005 and his Kit Fox Evaluation dated March 20, 2006, revised September 26, 2006:

BR-1 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during the breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grasslands. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged. Construction activities shall observe a 300 foot buffer for raptor nests.

BR-2 Prior to issuance of grading and/or construction permits, the applicant shall submit evidence to the City of Paso Robles (see contact information below) that states that one or a combination of the following four San Joaquin kit fox mitigation measures has been implemented:

- a. Provide for the protection in perpetuity, through acquisition of fee or a conservation easement of **93.9** acres of suitable habitat in the kit fox corridor area (e.g. within the San Luis Obispo County kit fox habitat area, northwest of Highway 58), either on-site or off-site, and provide for a non-wasting endowment to provide for management and monitoring of the property in perpetuity. Lands to be conserved shall be subject to the review and approval of the California Department of Fish and Game (Department) and the City.

This mitigation alternative (a.), requires that all aspects if this program must be in place before City permit issuance or initiation of any ground disturbing activities.

- b. Purchase **93.9** credits in a Department-approved conservation bank, which would provide for the protection in perpetuity of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring of the property in perpetuity.

At this time, there is no approved conservation bank that is operational in San Luis Obispo County. A conservation bank is expected to be operational in the near future. Purchase of credits must be completed prior to City permit issuance and initiation of any ground disturbing activities.

- c. If none of the above measures (a, b, or c) are available, the applicant may enter into a Mitigation Agreement with the Department, including depositing of funds into an

escrow account (or other means of securing funds acceptable to the Department) which would ensure the protection in perpetuity of **93.9** acres of suitable habitat within the kit fox corridor area and provide for a non-wasting endowment for management and monitoring in perpetuity. The Department can provide a draft agreement to review; a signed Mitigation Agreement shall be submitted to the City prior to City permit issuance and initiation of any ground disturbing activities.

BR-3 Prior to issuance of grading and/or construction permits, the applicant shall provide evidence that they have retained a qualified biologist acceptable to the City Planning Division. The retained biologist shall perform the following monitoring activities:

- a. **Prior to issuance of grading and/or construction permits and within 30 days prior to initiation of site disturbance and/or construction**, the biologist shall conduct a pre-activity (i.e. pre-construction) survey for known or potential kit fox dens and submit a letter to the City reporting the date the survey was conducted, the survey protocol, survey results, and what measures were necessary (and completed), as applicable, to address any kit fox activity within the project limits.
- b. The qualified biologist shall conduct weekly site visits during site-disturbance activities (i.e. grading, disking, excavation, stock piling of dirt or gravel, etc.) that proceed longer than 14 days, for the purpose of monitoring compliance with required Mitigation Measures BR-3 through BR11. Site- disturbance activities lasting up to 14 days do not require weekly monitoring by the biologist unless observations of kit fox or their dens are made on-site or the qualified biologist recommends monitoring for some other reason (see BR-2-c3). When weekly monitoring is required, the biologist shall submit weekly monitoring reports to the City.
- c. **Prior to or during project activities**, if any observations are made of San Joaquin Kit fox, or any known or potential San Joaquin kit fox dens are discovered within the project limits, the qualified biologist shall re-assess the probability of incidental take (e.g. harm or death) to kit fox. At the time a den is discovered, the qualified biologist shall contact the U.S. Fish and Wildlife Service and the Department for guidance on possible additional kit fox protection measures to implement and whether or not a Federal and/or State incidental take permit is needed. If a potential den is encountered during construction, work shall stop until such time the U.S. Fish and Wildlife Service/Department determine it is appropriate to resume work.

If incidental take of kit fox during project activities is possible, **before project activities commence**, the applicant must consult with the U.S. Fish and Wildlife Service and the Department (see contact information below). The results of this consultation may require the applicant to obtain a Federal and/or State permit for incidental take during project activities. The applicant should be aware that the

presence of kit foxes or known or potential kit fox dens at the project site could result in further delays of project activities.

In addition, the qualified biologist shall implement the following measures:

1. **Within 30 days prior to initiation of site disturbance and/or construction**, fenced exclusion zones shall be established around all known and potential kit fox dens. Exclusion zone fencing shall consist of either large flagged stakes connected by rope or cord, or survey laths or wooden stakes prominently flagged with survey ribbon. Each exclusion zone shall be roughly circular in configuration with a radius of the following distance measured outward from the den or burrow entrances:

- a) Potential kit fox den: 50 feet
- b) Known or active kit fox den: 100 feet
- c) Kit fox pupping den: 150 feet

2. All foot and vehicle traffic, as well as all construction activities, including storage of supplies and equipment, shall remain outside of exclusion zones. Exclusion zones shall be maintained until all project-related disturbances have been terminated, and then shall be removed.

3. If kit foxes or known or potential kit fox dens are found on site, daily monitoring during ground disturbing activities shall be required by a qualified biologist.

BR-4 Prior to issuance of grading and/or construction permits, the applicant shall clearly delineate as a note on the project plans, that: *“Speed signs of 25 mph (or lower) shall be posted for all construction traffic to minimize the probability of road mortality of the San Joaquin kit fox”*. Speed limit signs shall be installed on the project site **within 30 days prior to initiation of site disturbance and/or construction**,

In addition, **prior to permit issuance and initiation of any ground disturbing activities**, conditions BR-3 through BR-11 of the Developer's Statement/Conditions of Approval shall be clearly delineated on project plans.

BR-5 During the site disturbance and/or construction phase, grading and construction activities after dusk shall be prohibited unless coordinated through the City, during which additional kit fox mitigation measures may be required.

BR-6 Prior to issuance of grading and/or construction permit and within 30 days prior to initiation of site disturbance and/or construction, all personnel associated with the project shall attend a worker education training program, conducted by a qualified biologist, to avoid or reduce impacts on sensitive biological resources (i.e. San Joaquin kit fox). At a minimum, as the program relates to the kit fox, the training shall

include the kit fox's life history, all mitigation measures specified by the City, as well as any related biological report(s) prepared for the project. The applicant shall notify the City shortly prior to this meeting. A kit fox fact sheet shall also be developed prior to the training program, and distributed at the training program to all contractors, employers and other personnel involved with the construction of the project.

BR-7 During the site-disturbance and/or construction phase, to prevent entrapment of the San Joaquin kit fox, all excavation, steep-walled holes or trenches in excess of two feet in depth shall be covered at the close of each working day by plywood or similar materials, or provided with one or more escape ramps constructed of earth fill or wooden planks. Trenches shall also be inspected for entrapped kit fox each morning prior to onset of field activities and immediately prior to covering with plywood at the end of each working day. Before such holes or trenches are filled, they shall be thoroughly inspected for entrapped kit fox. Any kit fox so discovered shall be allowed to escape before field activities resume, or removed from the trench or hole by a qualified biologist and allowed to escape unimpeded.

BR-8 During the site-disturbance and/or construction phase, any pipes, culverts, or similar structures with a diameter of four inches or greater, stored overnight at the project site shall be thoroughly inspected for trapped San Joaquin kit foxes before the subject pipe is subsequently buried, capped, or otherwise used or moved in any way. If during the construction phase a kit fox is discovered inside a pipe, that section of pipe will not be moved, or if necessary, be moved only once to remove it from the path of activity, until the kit fox has escaped.

BR-9 During the site-disturbance and/or construction phase, all food-related trash items such as wrappers, cans, bottles, and food scraps generated shall be disposed of in closed containers only and regularly removed from the site. Food items may attract San Joaquin kit foxes onto the project site, consequently exposing such animals to increased risk of injury or mortality. No deliberate feeding of wildlife shall be allowed.

BR-10 Prior to, during and after the site-disturbance and/or construction phase, use of pesticides or herbicides shall be in compliance with all local, state and federal regulations. This is necessary to minimize the probability of primary or secondary poisoning of endangered species utilizing adjacent habitats, and the depletion of prey upon which San Joaquin kit foxes depend.

BR-11 During the site-disturbance and/or construction phase, any contractor or employee that inadvertently kills or injures a San Joaquin kit fox or who finds any such animal either dead, injured, or entrapped shall be required to report the incident immediately to the applicant and City. In the event that any observations are made of injured or dead kit fox, the applicant shall immediately notify the U.S. Fish and Wildlife Service and the Department by telephone (see contact information below). In addition,

formal notification shall be provided in writing within three working days of the finding of any such animal(s). Notification shall include the date, time, location and circumstances of the incident. Any threatened or endangered species found dead or injured shall be turned over immediately to the Department for care, analysis, or disposition.

BR-12 Prior to final inspection, or occupancy, whichever comes first, should any long internal or perimeter fencing be proposed or installed, the applicant shall do the following to provide for kit fox passage:

- a. If a wire strand/pole design is used, the lowest strand shall be no closer to the ground than 12".
- b. If a more solid wire mesh fence is used, 8" x 12" openings near the ground shall be provided every 100 yards.

Upon fence installation, the applicant shall notify the City to verify proper installation. Any fencing constructed after issuance of a final permit shall follow the above guidelines.

BR-13 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grass lands. If nesting birds are located, no construction activities shall occur within 100 feet of nest until chicks are fledged. Construction activities shall observe a 300 foot buffer for raptor nests.

Contact Information

California Department of Fish and Game
Central Coast Region
P.O. Box 47
Yountville, CA 94599
(805) 528-8670

U.S. Fish and Wildlife Service
Ventura Field Office
2493 Portola Road, Suite B
Ventura, CA 93003
(805) 644-1766

City of Paso Robles
Planning Department

Darren Nash
1000 Spring Street
Paso Robles, CA 93446
(805) 237-3970

BR-13 A pre-construction survey shall be conducted within thirty days of beginning work on the site to identify if badgers are using the site. The results of the survey shall be sent to the project manager, CDFG, and the City of Paso Robles.

If the pre-construction survey finds potential badger dens, they shall be inspected to determine whether they are occupied. The survey shall cover the entire property, and shall examine both old and new dens. If potential badger dens are too long to completely inspect from the entrance, a fiber optic scope shall be used to examine the den to the end. Inactive dens may be excavated by hand with a shovel to prevent re-use of dens during construction. If badgers are found in dens on the property between February and July, nursing young may be present. To avoid disturbance and the possibility of direct take of adults and nursing young, and to prevent badgers from becoming trapped in burrows during construction activity, no grading shall occur within 100 feet of active badger dens between February and July. Between July 1st and February 1st all potential badger dens shall be inspected to determine if badgers are present. During the winter badgers do not truly hibernate, but are inactive and asleep in their dens for several days at a time. Because they can be torpid during the winter, they are vulnerable to disturbances that may collapse their dens before they rouse and emerge. Therefore, surveys shall be conducted for badger dens throughout the year. If badger dens are found on the property during the pre-construction survey, the CDFG wildlife biologist for the area shall be contacted to review current allowable management practices.

BR-14 Prior to removal of any trees over 20 inches dbh, a survey shall be conducted by a qualified biologist to determine if any of the trees proposed for removal harbor sensitive bat species or maternal bat colonies. Maternal bat colonies may not be disturbed.

BR-15 Immediately prior to ground disturbance activities, if work occurs between March 1st and August 1st, nesting bird surveys shall be conducted. To avoid impacts to nesting birds, grading and construction activities that affect grassland habitats or trees shall not be conducted during the breeding season from March 1st to August 1st. If construction activities must be conducted during this period, nesting bird surveys shall take place within one week of ground disturbance. Ground nesting birds are possible in un-mowed fallow grasslands. If surveys do not locate nesting birds, construction activities may be conducted. If nesting birds are located, no construction activities shall occur within 100 feet of nests until chicks are fledged.

BR-16 Prior to commencement of project grading, protocol level surveys shall be conducted by a qualified biologist to determine if any listed species of fairy shrimp occur in ephemeral pools on the property. If vernal pool fairy shrimp are discovered, consultation with the US Fish and Wildlife Service must occur.

BR-17 Prior to commencement of project grading, a survey shall be conducted by a qualified biologist to determine if spadefoot toads are breeding in ephemeral pools on the property. Surveys shall be conducted from January to April, as conditions permit. If Western spadefoot toad are discovered, consultation with CDFG shall occur.

BR-18 Minimize impacts to rare species habitat. If less than 10 percent of any one plant population is impacted, no effect on the viability of the population is expected.

BR-19 If 10 percent or more of any one population is impacted, replace impacted rare species habitat in kind elsewhere on the property. Prepare a mitigation and monitoring plan for rare species habitat replacement on site.

- Obispo Indian paintbrush is an annual wildflower, the seeds of which can be collected in early May and sowed in proposed open space areas.

Airport Related Conditions:

14. The specific boundaries of the Airport Zones shall be overlaid over the tract map and be recorded with the Tract Map. An exhibit along with Table 10 and associated notes from the Airport Land Use Plan shall be recorded against each parcel within the subdivision.

15. All development projects on all lots created by a subdivision will require constructive notice also known as a Disclosure Document to be recorded on each parcel notifying future property owners that each parcel will need to be developed in accordance with the Airport Land Use Plan and meet all requirements set by the ALUC.

The maximum non-residential density allowed per acre on each parcel shall conform to the recommended densities of the APLUP. A Disclosure Document will ensure that all owners, potential purchasers, occupants (whether as owners or renters) receive full and accurate disclosure concerning the noise, safety, or overflight impacts associated with airport operations prior to entering any contractual obligation to purchase, lease, rent, or otherwise occupy any property or properties within the airport area.

16. Avigation easements shall be recorded for all properties created by any associated subdivision of the subject parcel. If no subdivision takes place any future development will be required to obtain and record the appropriate avigation easements.

17. Uses listed as noise sensitive uses by the ALUP will be prohibited.

Engineering Site Specific Conditions

18. Dry Creek Road shall be constructed in accordance with the typical cross-section shown on the Tentative Map with a center median and one lane in each direction, within the boundaries of the subdivision in accordance with plans approved by the City Engineer.
19. A temporary extension of Dry Creek Road must be completed in accordance with plans approved by the City Engineer in the remainder parcel from the west boundary of Phase I to tie to the existing alignment of Dry Creek Road in order to maintain access to properties to the west. The alternate Dry Creek Road section shown on the tentative tract map will be appropriate for this section of roadway.
20. Airport Road must be improved to provide a left turn pocket for access to Dry Creek Road in accordance with plans approved by the City Engineer.
21. With each building permit, each development within Tract 2772 will provide its share of the cost of the dual left turn project at the intersection of Highways 101 and 46 East.
22. With each building permit, each development within Tract 2772 will provide its share of the cost of improvements to Airport Road from Dry Creek Road to Highway 46 E.
23. With each building permit, each development within Tract 2772 will provide its share of the cost on intersection improvements at Airport Road and Highway 46 East.
24. Constructive notice shall be filed on the Remainder Parcel that consideration of access to properties to the south must be included in any development application.
25. A 12-inch sewer main shall be extended from the Airport trunk line to the east boundary of Tract 2772 generally in accordance with the master plan of the Dry Creek Road sewer main on file in the City Public Works Department and in accordance with plans approved by the City Engineer.
26. A storm water quality and erosion control basin shall be designed and constructed to serve Tract 2772 in accordance with plans approved by the City Engineer to collect all dry season run-off from landscape irrigation in parkways and the developed lots.
27. A parkway and median planting master plan shall be provided with improvement plans for Tract 2772. All irrigation and related infrastructure shall be placed with the construction of improvements in Tract 2772. Landscape planting shall be installed with the development of each lot.

Emergency Services Conditions

28. Provide fire sprinkler systems for residential, commercial, and industrial buildings.

29. Provide secondary emergency vehicle access sufficient to support the City's fire apparatus (HS-20 Truck Loading). Secondary vehicle access to be at least twenty (20) feet wide with no less than thirteen feet, six inches vertical clearance. All secondary emergency vehicle access surfaces shall provide all weather driving capabilities and conform to the requirements of City Zoning Codes.
30. Prior to the start of construction, documentation shall be submitted to Emergency Services showing that required fire flows can be provided to meet all project demands.
31. Provide an approved turn-around at the end of "A Street".

PASSED AND ADOPTED THIS 10th day of October, 2006 by the following Roll Call Vote:

AYES:
NOES:
ABSENT:
ABSTAIN:

CHAIRMAN, JOHN HAMON

ATTEST:

RON WHISENAND, SECRETARY OF THE PLANNING COMMISSION

H:\darren\PD\Mondo\Tract Map Reso

EXHIBIT A OF RESOLUTION 06-_____

**CITY OF EL PASO DE ROBLES STANDARD DEVELOPMENT CONDITIONS
FOR SINGLE FAMILY RESIDENTIAL TRACT AND PARCEL MAPS**

PROJECT #: Tentative Tract 2772 & PD 05-016

APPROVING BODY: Planning Commission

DATE OF APPROVAL: October 10, 2006

APPLICANT: Airport Rd. LLC - Doug Mondo

LOCATION: Dry Creek Road-Airport Road

The following conditions that have been checked are standard conditions of approval for the above referenced project. The checked conditions shall be complied with in their entirety before the project can be finalized, unless otherwise specifically indicated. In addition, there may be site specific conditions of approval that apply to this project in the resolution.

COMMUNITY DEVELOPMENT DEPARTMENT - The applicant shall contact the Planning Division, (805) 237-3970, for compliance with the following conditions:

A. GENERAL CONDITIONS

- 1. This project approval shall expire on October 10, 2008, unless a time extension request is filed with the Community Development Department prior to expiration.
- 2. The site shall be developed and maintained in accordance with the approved plans and unless specifically provided for through the Planned Development process, development shall comply with the Zoning Code, all other applicable City Ordinances, and applicable Specific Plans.
- 3. Prior to recordation of the map, all conditions of approval shall be completed to the satisfaction of the City Engineer and Community Developer Director or his designee.
- 4. This project is subject to the California Environmental Quality Act (CEQA), which requires the applicant submit a \$25.00 filing fee for the Notice of Determination payable to "County of San Luis Obispo". The fee should be submitted to the Community Development Department within 24 hours of project approval, which is then forwarded to the San Luis Obispo County Clerk. Please note that the project may be subject to court challenge unless the required fee is paid.
- 5. In accordance with Government Section 66474.9, the subdivider shall defend, indemnify and hold harmless the City, or its agent, officers and employees, from any claim, action or proceeding brought within the time period provided for in Government Code section 66499.37, against the City, or its agents, officers, or employees, to attack, set aside, void, annul the City's approval of this subdivision. The City will promptly notify subdivider of any such claim or action and will cooperate fully in the defense thereof.

(Adopted by Planning Commission Resolution 94-038)

- 6. All signs shall be subject to review and approval as required by Municipal Code Section 21.19 and shall require a separate application and approval prior to installation of any sign.
- 7. All existing and/or new lighting shall be shielded so as to be directed downward in such a manner as to not create off-site glare or adversely impact adjacent properties. The style, location and height of the lighting fixtures shall be submitted with the building plans and subject to approval by the Community Development Department.
- 8. All existing and/or new landscaping shall be installed with automatic irrigation systems.
- 9. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 10. The following areas shall be placed in a Landscape and Lighting District:
NONE
- 11. The following areas shall be permanently maintained by the property owner, Homeowners' Association, or other means acceptable to the City:

_____.
- 12. The applicant shall install durable, decorative fence/wall treatments and landscaping along all arterial streets consisting of brick, tubular steel with pilasters, or other similar materials as determined by the Development Review Committee, but specifically excluding precision block and wood fences. Substantial setbacks with landscaping may be considered as an alternative, subject to approval by the Development Review Committee.
- 13. The applicant shall provide a one-foot non-access easement along the rear/side of all lots that back up/side against a collector or arterial street.

B. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO THE ISSUANCE OF BUILDING PERMITS OR RECORDATION OF THE FINAL MAP, WHICHEVER OCCURS FIRST:

- 1. Two sets of the revised Planning Commission approved plans incorporating all Conditions of Approval, standard and site specific, shall be submitted to the Community Development Department.
- 2. Prior to the issuance of building permits, the
 - Development Review Committee shall approve the following:
 - Planning Division Staff shall approve the following:
 - a. A detailed landscape plan including walls/fencing;
 - b. Other: Exterior Lighting Cut Sheets
- 3. The Covenants, Conditions, and Restrictions (CC&Rs) and/or Articles Affecting Real Property Interests are subject to the review and approval of the Community Development Department, the

(Adopted by Planning Commission Resolution 94-038)

Public Works Department and/or the City Attorney. They shall be recorded concurrently with the Final Map or prior to the issuance of building permits, whichever occurs first. A recorded copy shall be provided to the affected City Departments.

- 4. The applicant shall agree, in a manner acceptable to the City Attorney, to pay impact mitigation fees as may be established through a resolution or ordinance adopted by the City Council, in effect at the time building permits are issued.

- N/A 5. ~~In order for this tract/parcel map to be in conformance with the General Plan, the lots/parcels of the tract/parcel map shall be annexed into a Community Facilities District (CFD) that serves to mitigate impacts to public schools. Said CFD shall either be a joint City School District CFD or a CFD created by the School District that the City Council has approved. If at the time that the final map is submitted for approval, proceedings to annex the tract/parcel map into a CFD have not been completed, the applicant shall record on all lots/parcels, a waiver of future protest to the formation of a CFD joint City School District CFD of a CFD created by the School Districts that the City Council has approved. This condition shall not be imposed if the developer executes a development agreement with the District to mitigate school impacts.~~

- 6. Street names shall be submitted for review and approval by the Planning Commission, prior to approval of the final map.

- 7. The developer shall provide constructive notice to all buyers that all homes are required to utilize semi-automated trash containers as provided by the City's franchisee for solid waste collection.

- 8. The developer shall provide constructive notice to future buyers that all residential units shall be required to be equipped with trash compactors.

- 9. The applicant shall meet with the City's Crime Prevention Officer prior to the issuance of building permits for recommendations on security measures to be incorporated into the design of the structures to be constructed. The applicant is encouraged to contact the Police Department at (805) 237-6464 prior to plan check submittal.

(Adopted by Planning Commission Resolution 94-038)

PUBLIC WORKS DEPARTMENT - The applicant shall contact the Engineering Division, (805) 237-3860, for compliance with the following conditions:

APPLICANT: Doug Mondo
REPRESENTATIVE: McCarthy
PROJECT: Tentative Tract 2772

PREPARED BY: John Falkenstien
CHECKED BY:
TO PLANNING:

C. PRIOR TO ANY PLAN CHECK:

- 1. The applicant shall enter into an Engineering Plan Check and Inspection Services Agreement with the City.

D. PRIOR TO RECORDING OF THE FINAL OR PARCEL MAP:

- 1. The owner shall pay all Final Map fees, and current and outstanding fees for Engineering Plan Check and Construction and Inspection services and any annexation fees due.
- 2. If, at the time of approval of the final/record parcel map, any required public improvements have not been completed and accepted by the City the owner shall be required to enter into a Subdivision Agreement with the City in accordance with the Subdivision Map Act, prior to recordation. The owner shall also be required to post securities to guarantee the installation and completion of said improvements as specified in the Subdivision Map Act and submit a Certificate of Insurance as required by the City. The owner shall also be required to post securities for grading in accordance with Section 7008 of the Uniform Building Code, latest edition. This bond shall be of sufficient amount to ensure completion of the grading and drainage facilities. (A finding of "orderly development" has been made for this condition on parcel maps).

Bonds required and the amount shall be as follows:
Performance Bond.....100% of improvement costs.
Labor and Materials Bond.....50% of performance bond.

- 3. The developer shall annex to the City's Landscape and Lighting District for payment of the operating and maintenance costs of the following:
 - a. Street lights;
 - b. Parkway and open space landscaping;
 - c. Wall maintenance in conjunction with landscaping;
 - d. Graffiti abatement;
 - e. Maintenance of open space areas.
- 4. The owner shall offer to dedicate to the City a 6 foot public utilities and 6 foot tree easement adjacent to all road right-of-ways. The owner shall offer to dedicate to the City the following easement(s). The location and alignment of the easement(s) shall be to the description and satisfaction of the City Engineer:
 - a. Public Utilities Easement;
 - b. Water Line Easement;
 - c. Sewer Facilities Easement;
 - d. Landscape Easement;
 - e. Storm Drain Easement.

(Adopted by Planning Commission Resolution 94-038)

- 5. The subdivider shall offer to dedicate and improve the following street(s) to the standard indicated:
- 6. Landscape and irrigation plans for the public right-of-way shall be incorporated into the improvement plans and shall require a signature of approval by the Department of Public Works, Street Superintendent and the Community Development Department.
- 7. All improvement plans shall be prepared by a registered civil engineer and shall be submitted to the City Engineer for review and approval. The improvements shall be designed and placed to Public Works Department Standards and Specifications.
- 8. Prior to any site work a Preliminary Soils Report shall be prepared for the property to determine the presence of expansive soils or other soils problems and shall make recommendations regarding grading of the proposed site.
- 9. The applicant shall submit a composite utility plan signed as approved by a representative of each public utility, together with the improvement plans. The composite utility plan shall also be signed by the Water, Fire, Wastewater and Street Division Managers.
- 10. A complete grading and drainage plan prepared by a registered civil engineer shall be included with the improvement plans. Drainage calculations shall be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer.
- 11. The owner shall provide an additional map sheet to record concurrently with the final map or parcel map showing the lot configuration, and the area subject to inundation by the 100 year storm with base flood elevations shown in feet, in relation to the National Geodetic Vertical Datum of 1929.
- 12. The owner shall install all utilities (sewer, water, gas, electricity, cable TV, and telephone) underground to each lot in the subdivision. Street lights shall be installed at locations as required by the City Engineer. All existing overhead utilities adjacent to or within the project shall be relocated underground, except for electrical lines 77 kilovolts or greater. All utilities shall be extended to the boundaries of the project, unless it is determined that no need for future extension exists. All underground construction shall be completed and approved by the City and the public utility companies, and the subgrade shall be scarified and compacted, before paving the streets.
- 13. Any utility trenching in existing streets shall be overlaid to restore a smooth riding surface as required by the City Engineer. Boring and jacking rather than trenching may be required on newly constructed or heavily traveled City Streets.
- 14. Prior to paving any street, the water and sewer systems shall successfully pass a City pressure test. The sewer system shall also be tested by a means of a mandrel and video inspection with a copy of the video tape provided to the City. No paving shall occur until the City has reviewed and viewed the sewer video tape and has determined that the sewerline is acceptable. Any repair costs to the pipeline including trench paving restoration shall be at the developer's expense.
- 15. The owner shall install all street name, traffic signs and traffic striping as directed by the City Engineer.
- 16. The adjoining existing City street is inadequate for the traffic generated by the project, or will be severely damaged by the construction. The applicant shall remove the entire roadway and replace it

(Adopted by Planning Commission Resolution 94-038)

with a minimum full half-width street plus a 12' wide travel lane and 8' wide base shoulder adequate to provide for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition.)

- 17. The development includes a phased street construction along the project boundary for future completion by the adjacent property owner, the applicant shall provide a minimum half-width street plus a 12' travel lane and 4' wide base shoulder adequate for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition.)
- 18. The project fronts on an existing street. The applicant shall pave-out from the proposed gutter to the edge of pavement if the existing pavement section is adequate, and shall feather the new paving out to the centerline for a smooth transition. If the existing pavement, structural sections or geometrics are inadequate per current City Standards, the roadway shall be replaced to centerline and the remaining pavement shall be overlaid. (A finding of "rough proportionality" has been made in the resolution for this condition.)

E. PRIOR TO ANY SITE WORK:

- 1. The applicant shall obtain a Grading Permit from the City Building Division.
- 2. Prior to issuance of a Grading Permit the developer shall apply, through the City, to FEMA and receive a Letter of Map Amendment (LOMA) issued from FEMA. The developer's engineer shall provide the required supporting data to justify the application.
- 3. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No. 553, Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to its removal.
- 4. All property corners shall be staked for construction control, and shall be promptly replaced if destroyed.
- 5. Any grading anticipated during the rainy season (October 15 to April 15) will require the approval of a construction zone drainage and erosion control plan to prevent damage to adjacent property. Appropriateness of areas shall be subject to City Engineer approval.
- 6. Any construction within an existing street shall require a traffic control plan. The plan shall include any necessary detours, flagging, signing, or road closures requested. Said plan shall be prepared and signed by a registered civil or traffic engineer.

F. PRIOR TO ISSUANCE OF A BUILDING PERMIT:

- 1. A final soils report shall be submitted to the City prior to the final inspection and shall certify that all grading was inspected and approved, and that all work has been done in accordance with the plans, preliminary report, and Chapter 70 of the Uniform Building Code.
- 2. The applicants civil and soils engineer shall submit a certification that the rough grading work has been completed in substantial conformance to the approved plans and permit.

- 3. Building permits shall not be issued until the water system has been completed and approved, and a based access road installed sufficient to support the City's fire trucks, in a manner approved by the Fire Chief.
- 4. Prior to issuance of a Building Permit for building within Flood Insurance Rate Map (FIRM) zones A1-A30, AE, AO, AH, A, V1-V30, VE and V, the developer shall provide an Elevation Certificate in accordance with the National Flood Insurance program. This form must be completed by a land surveyor, engineer or architect licensed in the State of California.
- 5. Prior to issuance of a Building Permit for building within Flood Insurance Rate Map (FIRM) zones A1-A30, AE, AO, AH, A, V1-V30, VE and V, the developer shall provide a Flood Proofing Certificate in accordance with the National Flood Insurance program. This form must be completed by a land surveyor, engineer or architect licensed in the State of California.

G. PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY:

- 1. All final property corners and street monuments shall be installed before acceptance of the public improvements.
- 2. No buildings shall be occupied until all public improvements are completed and approved by the City Engineer, and accepted by the City Council for maintenance.
- 3. All disturbed areas not slated for development shall be protected against erosion in a manner acceptable to the City Engineer, which may include hydroseeding or landscaping.
- 4. The applicant shall pay any current and outstanding fees for Engineering Plan Checking and Construction Inspection Services and any outstanding annexation fees.
- 5. All top soil removed shall be stockpiled and evenly distributed over the slopes and lots upon completion of rough grading to support hydroseeding and landscaping. All slope areas shall be protected against erosion by hydroseeding or landscaping.
- 6. All construction refuse shall be separated (i.e. concrete, asphalt concrete, wood, gypsum board, etc.) and removed from the project to a recycling facility in accordance with the City's Source Reduction and Recycling Element.
- 7. If any of the public improvements or conditions of approval are not completed or met, then the subdivider may, at the discretion of the City Engineer, enter into a Performance Agreement with the City to complete said improvements at a later date and post securities to cover the cost of the improvements. The form of the agreement and amount of the securities are subject to the approval of the City Engineer.
- 8. A blackline clear Mylar (0.4 MIL) copy and two (2) blue-line prints of as-built improvement plans, signed by the engineer of record, shall be provided to the City Engineer prior to the final inspection. A reduced copy (i.e. 1" = 100') of the composite utility plan shall be provided to update the City's Atlas Map.
- 9. A benchmark shall be placed for vertical control on the U.S.G.S. Datum as required by the City Engineer.

PASO ROBLES FIRE DEPARTMENT - The applicant shall contact the Fire Department, (805) 237-3973, for compliance with the following conditions:

(Adopted by Planning Commission Resolution 94-038)

H. GENERAL CONDITIONS

- 1. Fire hydrants shall be installed at intervals as required by the Fire Chief and City Engineer. The maximum spacing for single family residential shall be 500 feet. The maximum spacing for multi-family and commercial/residential shall be 300 feet. On-site hydrants shall be placed as required by the Fire Chief.
- 2. Building permits shall not be issued until the water system, including hydrants, has been tested and accepted and a based access road installed sufficient to support the City's fire apparatus (HS-20 truck loading). The access road shall be kept clear to a minimum of 24 feet at all times and shall be extended to each lot and shall be maintained to provide all weather driving conditions.
- 3. No buildings shall be occupied until all improvements are completed and accepted by the City for maintenance.
- 4. If the development includes phased street construction, temporary turn-arounds shall be provided for streets that exceed 150 feet in length. The temporary turn around shall meet City requirements as set forth in the Public Works Department Standards and Specifications.
- 5. All open space areas to be dedicated to the City shall be inspected by the Fire Department prior to acceptance. A report shall be submitted recommending action needed for debris, brush and weed removal and tree trimming. The developer shall clean out all debris, dead limbs and trash from areas to be recorded as open space prior to acceptance into a Benefit Maintenance District.
- 6. Any open space included in a private development shall be subject to the approval of a vegetation management plan approved by the Fire Chief.
- 7. Each tract or phase shall provide two sources of water and two points of access unless otherwise determined by the Fire Chief and Public Works Director.
- 8. Provisions shall be made to update the Fire Department Run Book.

TENTATIVE TRACT MAP 2772

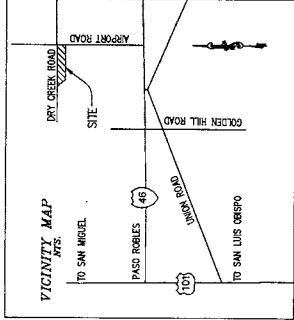
A SUBDIVISION OF PARCELS 1 AND 2 OF PR 04-078

IN THE CITY OF EL PASO DE ROBLES

TENTATIVE TRACT MAP 2772

BY THE CITY OF EL PASO DE ROBLES, COUNTY OF SAN JUAN, CALIFORNIA, IN ACCORDANCE WITH THE SUBDIVISION ACT, CHAPTER 4, ARTICLE 11, OF THE CONSTITUTION OF THE STATE OF CALIFORNIA.

PARCELS 1 AND 2 OF PR 04-078 AS RECORDED IN THE COUNTY OF SAN JUAN, CALIFORNIA, STATE OF CALIFORNIA.



General Notes

PROJECT - BUSINESS PARK PLANNED DEVELOPMENT AND SUBDIVISION.

PARCEL 1& 2 A PORTION OF LOT 2 OF PARCEL MAP PR 04-078.

TOTAL ACREAGE OF DEVELOPMENT AREA = 36.55 AC

36 LOTS PLUS LOT "A" FOR PRIVATE STREETS, RANGING FROM 0.25 ACRES TO 7 ACRES IN SIZE.

REVISION BY CHECK LOCATION 6/15/09

DATE



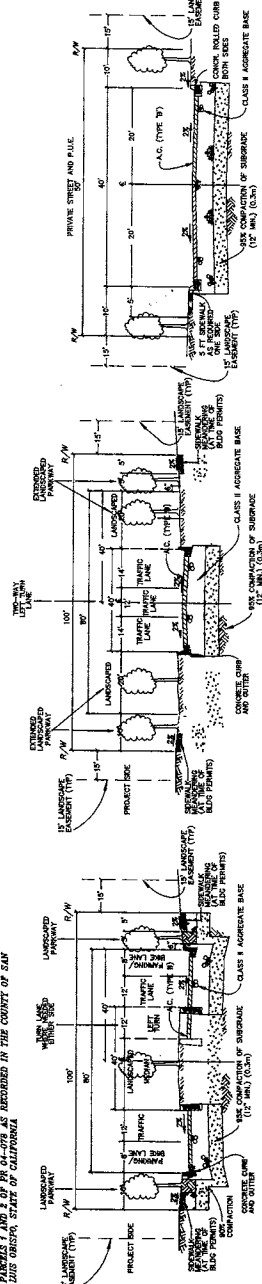
AIRPORT ROAD BUSINESS PARK

AIRPORT ROAD BUSINESS PARK MANAGEMENT LLC

Sheet 1 OF 7

Draw 8-9-06

Scale 1" = 100'

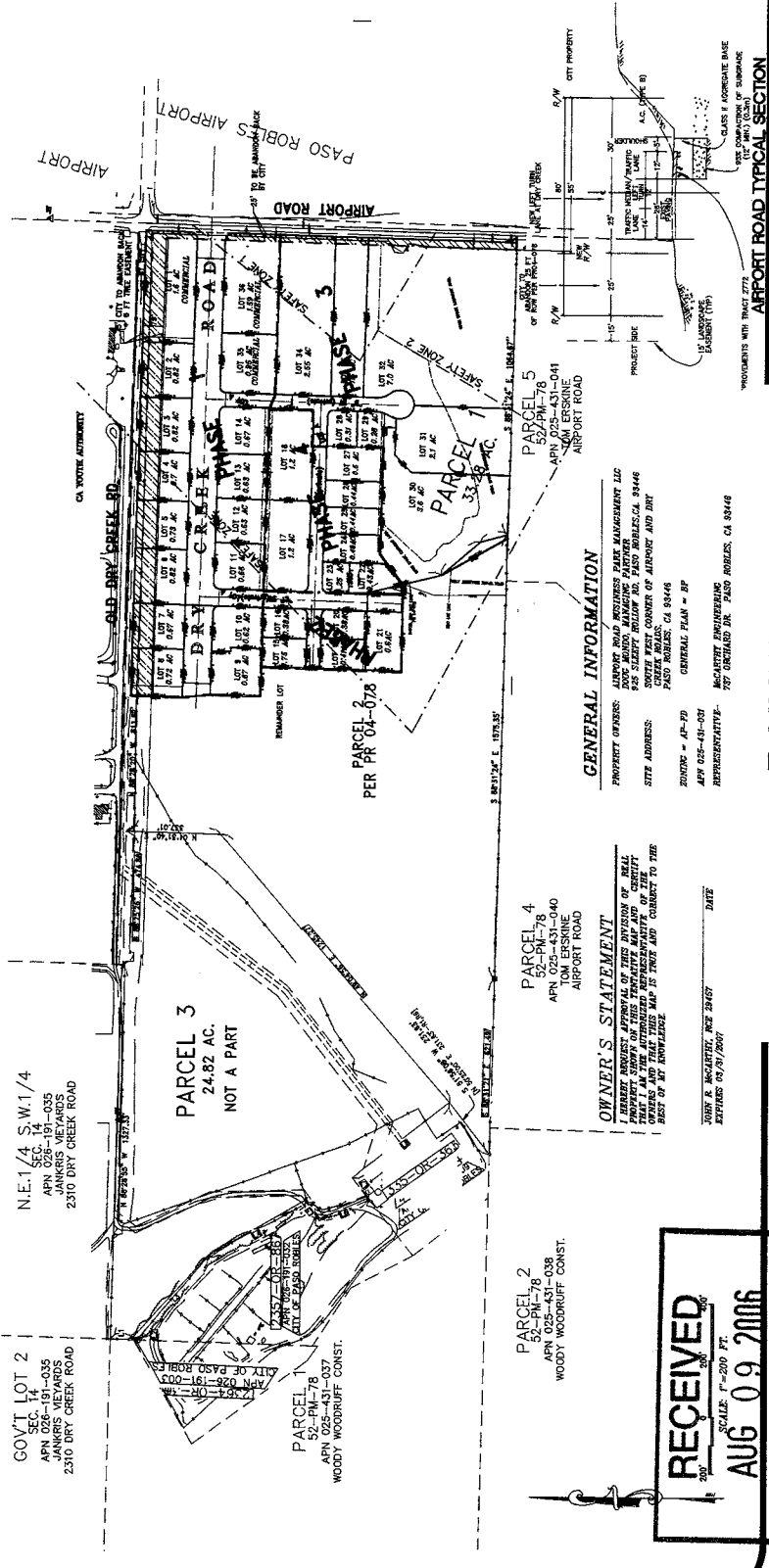


DRY CREEK ROAD TYPICAL SECTION

ALTERNATE ROAD SECTION

PRIVATE STREETS, A, B, C

LOT 'A' - COMMON PRIVATE STREET AREA



GOV'T LOT 2
SEC. 14
APN 025-181-035
JANKRIS VIEYARDS
2310 DRY CREEK ROAD

N.E. 1/4 S.W. 1/4
APN 025-191-035
JANKRIS VIEYARDS
2310 DRY CREEK ROAD

PARCEL 3
24.82 AC.
NOT A PART

PARCEL 4
52-PM-78
APN 025-431-040
TOM ERSKINE
AIRPORT ROAD

PARCEL 5
52-PM-78
APN 025-431-041
TOM ERSKINE
AIRPORT ROAD

PARCEL 2-078
PER PR 04-078

PARCEL 1
52-PM-78
APN 025-431-037
WOODY WOODRUFF CONST.

PARCEL 3
33.38 AC

PARCEL 5
52-PM-78
APN 025-431-041
TOM ERSKINE
AIRPORT ROAD

OWNER'S STATEMENT

I HEREBY REQUEST APPROVAL OF THIS DIVISION OF REAL PROPERTY AND THE LOTS THEREON AND OF THE STREETS AND TRAIL THIS MAP IS TRAIL AND CORNER TO THE REST OF THE TRACT.

GENERAL INFORMATION

PROPERTY OWNERS: AIRPORT ROAD BUSINESS PARK MANAGEMENT LLC
500 MADONNA MANAGING PARTNER
835 SLEEPY BROTHER RD. PASO ROBLES, CA 94046

SITE ADDRESS: 835 SLEEPY BROTHER RD. PASO ROBLES, CA 94046

ZONING: APN-78
APN 025-431-031
REPRESENTATIVE:

RECEIVED

AUG 09 2006

SCALE: 1" = 200 FT

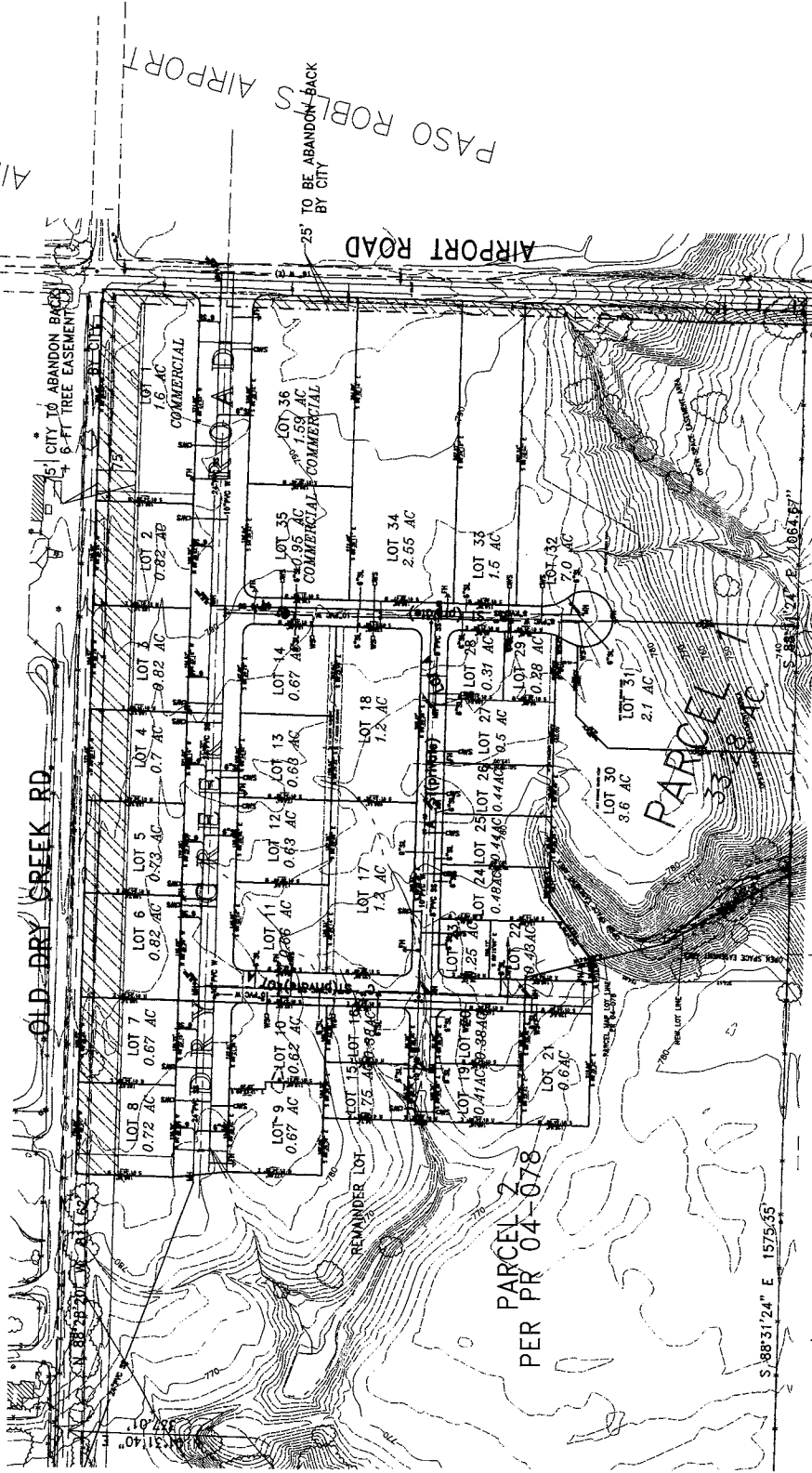
Engineering Division

Exhibit B

Tentative Tract Map
Tract 2772 & PD 05-016
(Mondo)

TENTATIVE TRACT MAP 2772
 IN THE CITY OF EL PASO DE ROBLES
 PRELIMINARY UTILITY PLAN

CA YOUTH AUTHORITY



General Notes	
No.	Revision/Issue
	Date

M&E Engineering, Inc.
 10000 Elgin Street, Suite 100
 Elgin, CA 95020
 (916) 781-5888 (cell) (916) 781-5888 fax

AIRPORT ROAD BUSINESS PARK
 MANAGEMENT LLC

UTILITY LAYOUT

Sheet: 3 OF 7

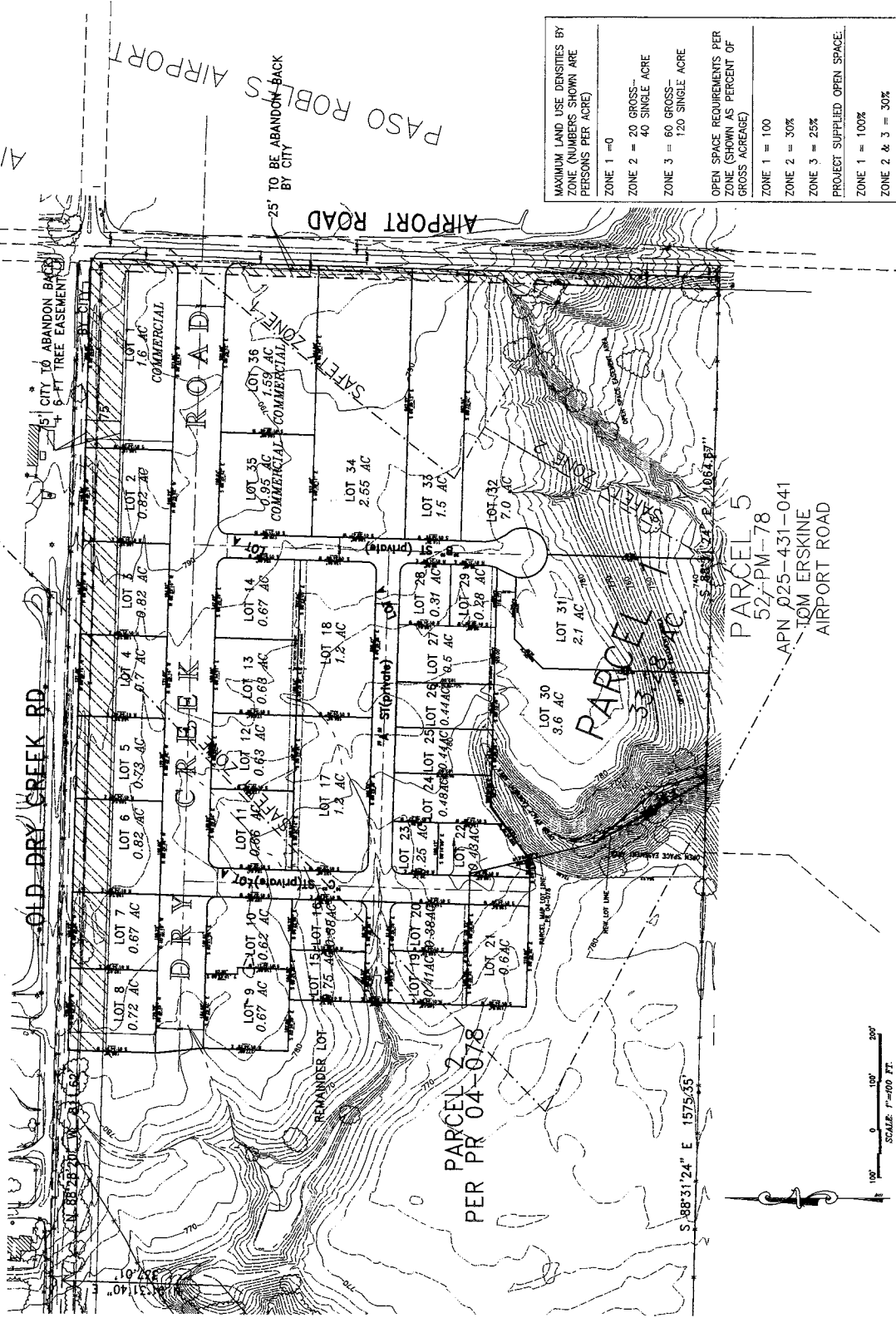
Date: 8-9-06

Scale: 1" = 100'

Exhibit D
 Utility Layout Plan
 Tract 2772 & PD 05-016
 (Mondo)

TENTATIVE TRACT MAP 2772
 IN THE CITY OF EL PASO DE ROBLES
 AIRPORT OVERLAY ZONES

CA YOUTH AUTHORITY



AIRPORT

PASO ROBLES AIRPORT

AIRPORT ROAD

DRY CREEK ROAD

Zone	Maximum Land Use Densities by Zone (Numbers Shown are Persons per Acre)
ZONE 1 = 0	0 PERSONS PER ACRE
ZONE 2 = 20 GROSS-40 SINGLE ACRE	20 PERSONS PER ACRE
ZONE 3 = 60 GROSS-120 SINGLE ACRE	60 PERSONS PER ACRE

Zone	Open Space Requirements per Zone (Shown as Percent of Gross Acreage)
ZONE 1 = 100	100%
ZONE 2 = 30%	30%
ZONE 3 = 25%	25%

PROJECT SUPPLIED OPEN SPACE:
 ZONE 1 = 100%
 ZONE 2 & 3 = 30%

<p>General Notes</p> <p>PROJECT - BUSINESS PARK</p> <p>COMPLETE 3 LOT PARCEL MAP, PREPARE CONCEPTS FOR DEVELOPMENT OF PARCEL 1.</p> <p>START ENVIRONMENTAL WORK FOR PARCEL ONE.</p> <p>KIT FOX STUDY --FISH AND GAME</p> <p>BIOLOGY REPORT ON SITE</p> <p>ARCHEOLOGIST REVIEW</p> <p>TRAFFIC REPORT</p>	
<p>PERMITTED MANUFACTURING PER EACH AIRPORT ZONE TYPE: (NOTE: ALL USES ARE SUBJECT TO DENSITY RESTRICTIONS FOR EACH ZONE.)</p> <p>ZONE 1 - VERY RESTRICTED, NONE ALLOWED, PARKING OR OPEN SPACE ONLY.</p> <p>ZONE 2 - PARKING OR OPEN SPACE ONLY, SOME WAREHOUSING MAY BE PERMITTED BY CITY.</p> <p>ZONE 3 - INDOOR AND OUTDOOR FABRICATION ALLOWED, WAREHOUSE AND WAREHOUSING ALLOWED, COMMERCIAL RETAIL</p>	
<p>No. _____</p> <p>Revision/Issue _____</p> <p>Date _____</p>	
<p>AIRPORT ROAD BUSINESS PARK</p>	
<p>Project No. 8-9-06</p> <p>Sheet 4 OF 7</p> <p>Scale 1" = 100'</p>	

Exhibit E
 Airport Overlay Zones
 Tract 2772 & PD 05-016
 (Mondo)

RESOLUTION NO.:

A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
TO APPROVE PLANNED DEVELOPMENT 05-016
(AIRPORT RD. BUSINESS PARK - MONDO)
APN: 025-431-031

WHEREAS, Tract 2772 has been filed by McCarthy Engineering on behalf of Airport Road Business Park, LLC – Doug Mondo, to subdivide a 39-acre property into 36 lots ranging in size from .25-acre to 7-acre parcels, for business park development; and

WHEREAS, Tract 2772 is located on the southwest corner of Airport Road and Dry Creek Road; and

WHEREAS, in conjunction with the Tract 2772, Planned Development 05-016 has been submitted to establish development standards for the business park; and

WHEREAS, at its October 10, 2006 meeting, the Planning Commission held a noticed public hearing on the Project, to accept public testimony on the proposal including Planned Development 05-016 and related applications; and

WHEREAS, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City's Procedures for Implementing CEQA, an Initial Study was prepared and circulated for public review and comment; and

WHEREAS, based on the information and analysis contained in the Initial Study, a determination has been made that the proposed Project qualifies for adoption of a Mitigated Negative Declaration; and

WHEREAS, based upon the facts and analysis presented in the staff report and the attachments thereto, the public testimony received, and subject to the Conditions of Approval listed below, the Planning Commission makes the following findings:

1. The proposed Project will not be detrimental to the City's efforts to revitalize Downtown Paso Robles since the Project is not in the downtown area would typically not have uses that would be found in the downtown area.
2. The proposed Planned Development is consistent with the purpose, intent and regulations set forth in Chapter 21.16A (Planned Development Overlay District Regulations) as follows:
 - A. The granting of the Planned Development (PD) will not adversely affect the policies, spirit and intent of the General Plan, the Zoning Ordinance, and the policies and plans of the City, by providing the opportunity for clean, attractive businesses to be located in the Business Park/Planned Industrial designated areas of the City.
 - B. The Project maintains and enhances the significant natural resources on the site. This has been accomplished through the use of extensive landscaping, and preservation of all oak trees on site.

- C. The Project is designed to be sensitive to, and blend in with, the character of the site and surrounding area. This has been accomplished through the conditions within the resolution along with the applicant's Development Design Guidelines (Exhibit D).
- D. The design and the density of the Project is compatible with the surrounding development and does not create a disharmonious or disruptive element to the neighborhood.
- E. The Project is consistent with the purpose and intent of the Planned Development Chapters of the Zoning Ordinance and the Project is not contrary to the public health, safety and welfare.

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of El Paso de Robles does hereby approve Planned Development 05-016, subject to the following conditions:

STANDARD CONDITIONS:

1. The Project shall comply with all Conditions of Approval and Exhibits contained in this Resolution and the associated Resolutions for the above-referenced Tract 2772. In the event that either the tract or development plan is not approved, the approval of one plan does not automatically grant approval of the other.

PLANNING SITE SPECIFIC CONDITIONS:

NOTE: In the event of conflict or duplication between standard and site-specific conditions, the site-specific condition shall supersede the standard condition.

2. The Project shall be constructed in substantial conformance with the Conditions of Approval established by this Resolution and it shall be constructed in substantial conformance with the following Exhibits:

EXHIBIT	DESCRIPTION
A	Tentative Tract Map/Site Plan
B	Conceptual Architectural Site Plan for Lots 2-10
C	Conceptual Typical Landscape Plans for Lots 2-10
D	Development Design Guidelines

3. This Development Plan for PD 05-016, together with the application for Tentative Tract Map 2772, allows for the subdivision and development of a 36-lot Business Park. The future development of each parcel will need to be reviewed by the Planning Commission via a Development Plan (PD). Please note that Exhibits B and C listed above are conceptual plans proposed by the Applicant, these conceptual plans may need to be changed as a result of going through the PD process.

4. Prior to issuance of a Building Permit for each individual site, the applicant shall apply for a Major Site Plan Review where the following information will need to be provided for review and approval by the Development Review Committee. The Site Plan submittal shall include but not be limited to the following items:
 - a. Site Plan (see application guide for requirements for a site plan)
 - b. Architectural elevations
 - c. Landscaping Plan
 - d. Grading and Drainage Plan
 - e. Color and Materials Board
 - f. Fencing Plans
 - g. Exterior Light Cut-Sheets
5. Prior to or in conjunction with the recording of Tract 2772, a constructive notice shall be recorded against each parcel notifying future property owners that Planned Development 05-016 has been approved for the development which establishes architectural, landscape and site development standards.
6. Prior to the recordation of the final map, the following details/plans shall be submitted for review by the DRC:
 - a. Master fencing plan for the business park which indicates the various fencing/decorative wall types that will be used throughout the business park.
 - b. Typical Landscape Plan for a interior lot and a corner lot.
 - c. Parkways and median landscape plan for Dry Creek Road.
7. Any outdoor storage shall be as an accessory to the on-site business and shall comply with the screening standards described in Section 21.21.110. Outdoor storage areas shall not back up to or be visible from Dry Creek Road or Airport Road.

PASSED AND ADOPTED THIS 10th day of October, 2006 by the following Roll Call Vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

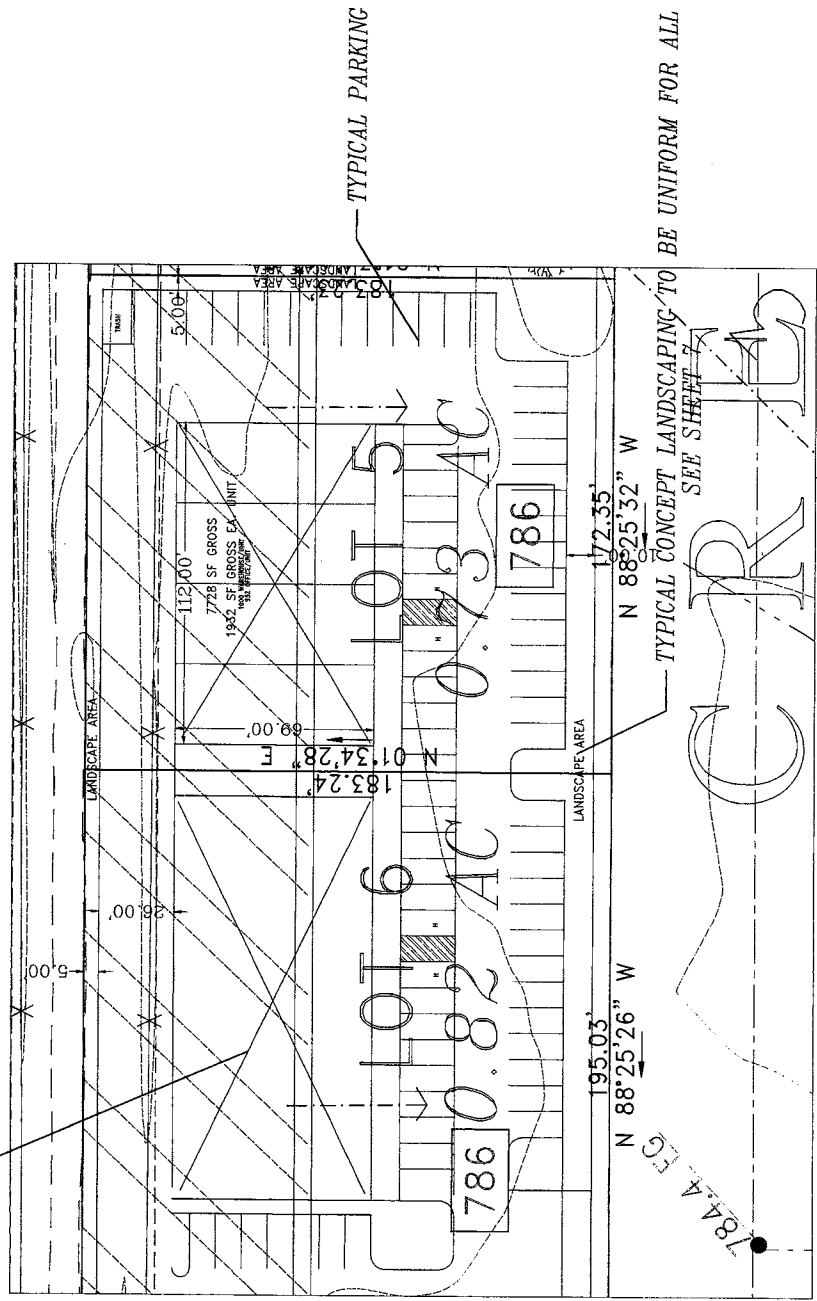
CHAIRMAN JOHN HAMON

ATTEST:

RON WHISENAND, PLANNING COMMISSION SECRETARY

TENTATIVE TRACT MAP 2772
 IN THE CITY OF EL PASO DE ROBLE
 TYPICAL CONCEPT DRAWINGS FOR PD, LOTS 2-10

NEW BUILDINGS TO CONFORM TO DEVELOPMENT STDS FOR TRACT
 WITH ARCHITECTURAL DETAILS AND COLORS



General Notes		Revision/Issue		Date
PROJECT - BUSINESS PARK		No.		


 TRACT 2772
 AIRPORT ROAD BUSINESS PARK
 MANAGEMENT LLC

Project	Sheet
8-9-06	6 of 7
AS SHOWN	

Exhibit B
 Concep. Arch. Site Plan (Lots 2-10)
 Tract 2772 & PD 05-016
 (Mondo)

TENTATIVE TRACT MAP 2772

IN THE CITY OF EL PASO DE ROBLES
TYPICAL LANDSCAPE DETAILS FOR PD, LOTS 2-10

NOTE: FOR COLOR VARIETY AND INTEREST, SUBSTITUTE AND MIX VARIETIES OF CISTUS OR ADD CERCIS OCCIDENTALIS 'ALBA' UNDER CERCIS OCCIDENTALIS.

PLANT LEGEND

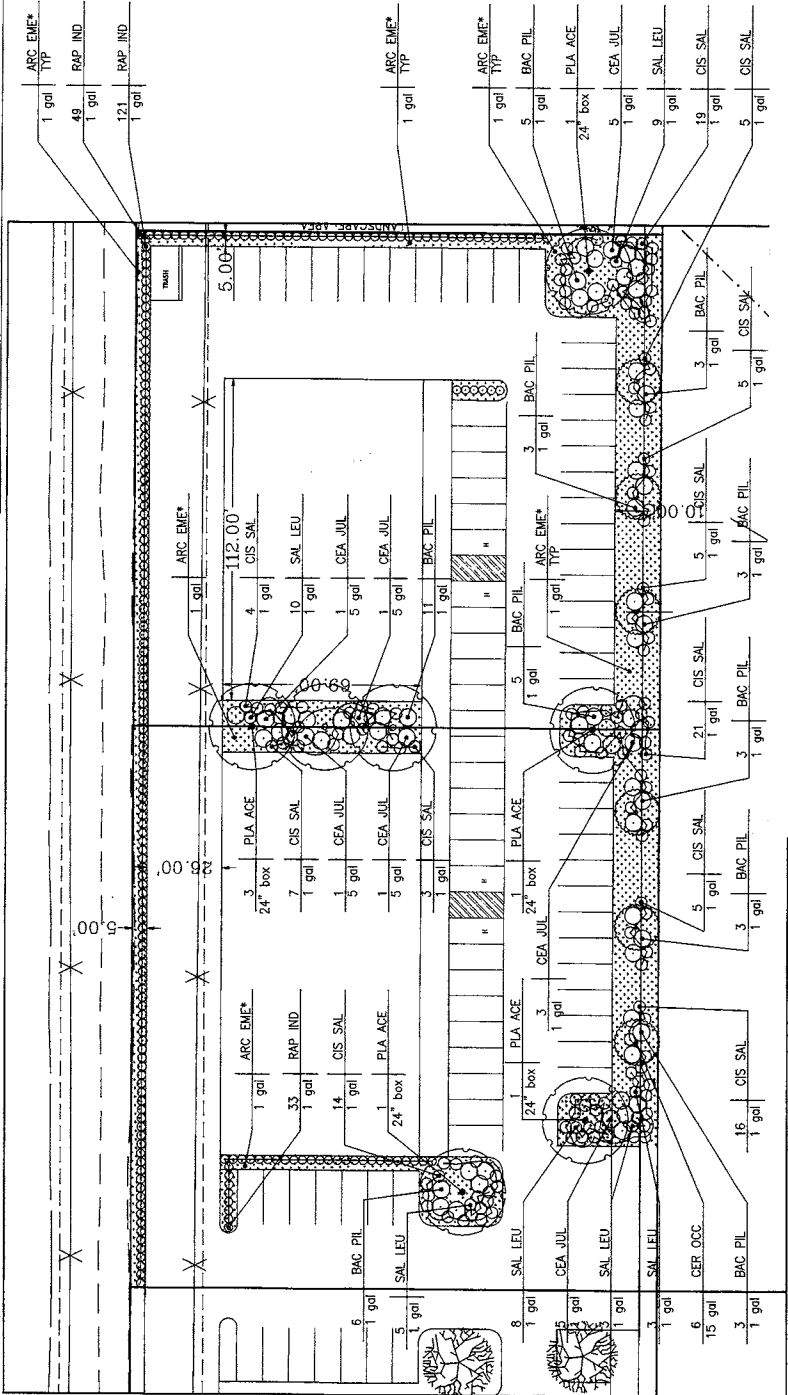
PROPOSED TREES

Sym.	Qty.	Size	Latin Name	Common Name	Growth Rate	HL
PLA ACE	7	24" Box	<i>Platanus x occidentalis</i>	London Plane	Mod	60'
CER OCC	6	15 gal	<i>Cercis occidentalis</i>	Western Redbud	Med.	16'

PROPOSED SHRUBS, PERENNIALS, & GROUNDCOVERS

Sym.	Qty.	Size	Latin Name	Common Name	Growth Rate	HL
ARC EME	1g		<i>Acacia saligna</i>	Emerald Carpet	Med.	12"
BAC PIL	45	1g	<i>Baccharis pilularis</i>	Dwarf Carpet Manzanita	Med.	2'
CEA JUL	17	5g	<i>Ceanothus 'Julio Phillips'</i>	Dwarf Conchita Bush	Slow	5'
CIS SAL	93	1g	<i>Cistus salicifolius</i>	White Lilac	Mod.	18"
SAL LEU	48	1g	<i>Salvia leucantha</i>	Sageleaf Rockrose	Fast	3'
RAP IND	239	1g	<i>Rapanea indica</i>	Mexican Sage	Fast	3'
			<i>Rapanea indica</i>	Indian Hawthorn	Med.	3'

* PLANT 3 FEET ON CENTER AT ALL HATCHED AREAS



General Notes
PROJECT - BUSINESS PARK

No.	Revision/Issue	Date



TRACT 2772
AIRPORT ROAD BUSINESS PARK
MANAGEMENT LLC
DEVELOPMENT PLAN
LANDSCAPE PLAN

Sheet	6-3-06	7 OF 7
Drawn	AS SHOWN	

Exhibit C
Concep. Landscape Site Plan (Lots 2-10)
Tract 2772 & PD 05-016
(Mondo)

DEVELOPMENT DESIGN GUIDELINES



for the

AIRPORT BUSINESS PARK

Paso Robles, California

DATE: January 23, 2006

PREPARED BY:

MCCARTHY ENGINEERING, INC.

PROJECT MANAGEMENT AND CIVIL ENGINEERING
737 ORCHARD DRIVE, PASO ROBLES, CALIFORNIA, 93446
(805)238-9585 (805)237-8556 FAX

Exhibit D

Development Design Guidelines
Tract 2772 & PD 05-016
(Mondo)



TABLE OF CONTENTS

PURPOSE AND INTENT - OVERVIEW

GENERAL DESIGN APPLICATIONS

I. SITE DESIGN

SETTING

SITE ORGANIZATION

OPEN SPACE

II. BUILDING AND ARCHITECTURAL DESIGN

SCALE AND CHARACTER

ACCESSORY UTILITY BUILDINGS AND TRASH ENCLOSURES

FENCES AND WALLS

ROOF

MATERIAL AND COLOR

III. PARKING AND CIRCULATION

IV. LANDSCAPING

V. APPENDIX

PURPOSE AND INTENT – OVERVIEW

The City of Paso Robles has identified various areas within the community for development of business and industrial land uses. The Airport Business Park development is located on Airport Road at Dry Creek road, adjacent to the Paso Robles Airport.

The intent of this document is to provide guidelines for development of the Airport Business Park that keep with the intent of the overall theme for a high-end business park. The elements in this document are also in keeping with the overall physical appearance for the Business Park as envisioned in the Paso Robles General Plan.

These guidelines are intended to support and complement the City's economic development goals and strategy. These guidelines provide advance data to architects and owners for the design elements necessary to develop in a cohesive nature into the overall theme of the Airport Business Park.

The document is setup to provide a set of design principles that can be applied in a flexible and results oriented manner while keeping within a "theme" for the overall development.

The intent of the document is to enhance the quality of the site and architectural design elements and to supplement but not substitute for existing City zoning standards and regulations.

General Design Applications

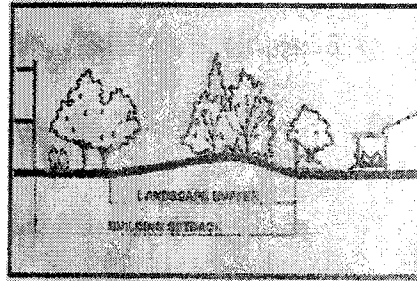
1. The Business Park Design Guidelines (BPDG) consist of two types of criteria: mandatory and recommended. The mandatory criteria are identified by the word “shall” and the recommended criteria are identified with the words “encouraged” or “should”.
2. The mandatory criteria are required elements of the project needed to obtain City approval of the proposed project. Omission or non-compliance with the mandatory criteria shall be cause of denial of the design concept.
3. Recommended criteria are strongly encouraged to be in the design pallet for the overall project, and will be encouraged by the City as elements desired for the project. If the recommended criteria are completely ignored for the majority of elements, this may also be cause for denial of the design concept.
4. These guidelines apply to the development known as the Airport Business Park developed under the subdivision map Tract 2772 in the City of Paso Robles.
5. All new construction, additions of any size visible to the public, all major exterior alterations of a building façade or changes to the roof line or parapet, modifications to parking, loading or mechanical equipment modifications that are visible to the public, shall comply with the applicable design criteria included in the BPDG.

I. SITE DESIGN

SETTING

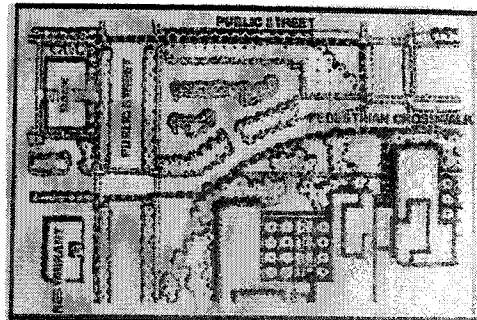
- A1. New lot development shall enhance the character of its surrounding area through quality architecture, landscaping, and appropriate site arrangement.
- A2. New development shall be compatible with its surrounding development in intensity, design, setback, building form, scale, material, color and landscaping.

- A3. Transitions shall be developed between projects with different uses and intensities on adjacent parcels to provide an effective “campus” feel. Transitions may be created through appropriate building setbacks, height and landscape buffers.



- A4. In multi-building complexes, a distinct visual link shall be established among various buildings by using site design elements such as courtyards, plazas, landscaping, and walkways to unify the project. Solar orientation of buildings is strongly encouraged in design.
- A5. Project perimeter landscaping shall be integrated with the landscaping of adjacent development for streetscape continuity.
- A6. Sidewalks shall be integrated into the existing frontage improvements and landscaping to maintain street continuity. Use of meandering sidewalks is encouraged.

- A7. Natural features on a site such as mature trees, creeks, views, etc. should be preserved and incorporated into the site design of the project.



- A8. Project sites should have convenient and safe pedestrian access to adjacent streets. On-site walkways should be linked to the public sidewalk system outside the project for ease of pedestrian access.

SITE ORGANIZATION

- B1. Site components such as structures, parking, driveways, and outdoor functions shall be arranged and located to emphasize the aesthetically pleasant components of the site such as natural drainage ways or views or superior architectural features. In a like manner less attractive scenes such as service areas, equipment areas and trash areas shall be disguised through placement and design of the structures and landscaping.
- B2. Location of noise and odor generating functions on the site shall not create a nuisance for the adjacent properties.
- B3. The on-site walkways shall link public outdoor areas with adjacent buildings. Use of textured concrete, paving, and brick or paver materials is encouraged along with a strong landscape emphasis.

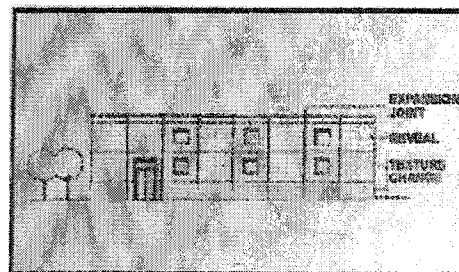
OPEN SPACE

- C1. Every project site shall be designated for maximum utility of open space for ventilation, sunlight, recreation and views for both new and existing buildings.
- C2. Incorporating benches, artwork landscaping, water and hardscape features into site open space areas is encouraged.
- C3. Airport Zones 1 and 2 are limited to open space and vehicle parking per the FAA requirements and the City adopted Airport Regulations. These zones are delineated on the Official tract map for the Business Park.

II. BUILDING AND ARCHITECTURAL DESIGN

SCALE AND CHARACTER

- A1. Professional Office buildings shall have the highest quality architecture and be oriented towards the street or interior courtyard.
- A2. Adjacent buildings on the same or separate parcels shall be compatible in height and scale. If different scale is required for

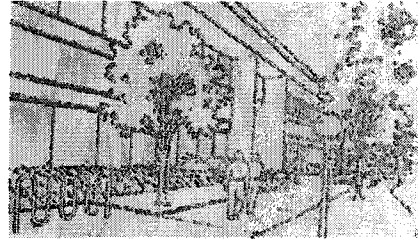


functional reasons, adequate transition shall be provided between buildings.

A3. Front facades of large buildings visible from a public street shall include architectural features such as reveals, windows and openings, expansion joints, changes in color, texture, and material to add interest to the building elevation.

A4. New buildings shall maintain individuality in style while keeping with the aesthetic character of the development.

A5. In multi-building complexes, a comprehensive architectural concept shall be developed and maintained. Various site components shall be unified through the use of similar design, materials and color.



A6. Buildings shall have façade articulation which may consist of changes in the wall plane, use of openings and projections, and material and color variations.

A7. Exterior articulation such as change in color, material, or plane shall also be a part of the overall design concept, as well as energy efficiency in siting of building orientation.

A8. Additional building setbacks with dense landscaping screening may also be used to screen buildings with less architecturally interesting facades where necessity dictates a more box-like form.

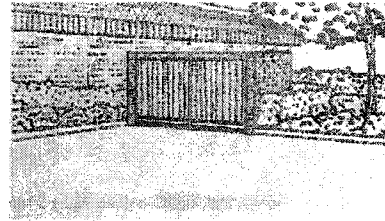
A9. Main entrances to the buildings shall be well defined. Public entrances and primary building elevations should face public streets where possible.

A10. New building shall have at least one major focal point with minor focal points. Focal points can be achieved through horizontal and vertical lines, change in material, change in color, change in the form and shape of a portion of the building, etc.

A11. Service doors shall be recessed and integrated into the overall design of the building.

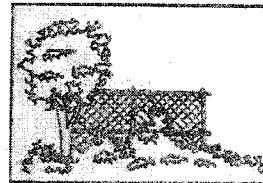
ACCESSORY UTILITY BUILDINGS AND TRASH ENCLOSURES

- B1. The design of accessory buildings shall be incorporated into and be compatible in design concept with the overall design of the project and the main buildings on the site.
- B2. Accessory buildings shall be located outside the front setback areas, defined as the area between the main building and the public right of way.
- B3. Temporary buildings and/or portable structures and trailers are not permitted except during site construction and must be removed as soon as construction is completed.
- B4. Trash enclosures shall be located so they are not visible from public rights of way and are easily accessible by collection trucks.
- B5. The design of the trash enclosures shall be compatible with the design of the main buildings on the site and shall be of slumpstone or split faced block or equivalent masonry construction.



FENCES AND WALLS

- C1. Fencing shall be other than chain link or PVC, and shall have decorative qualities. Masonry fencing is encouraged with split faced or slumpstone block.
- C2. Fencing shall not be located within the front setback area of the building.



ROOF

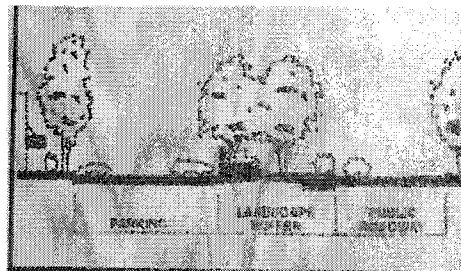
- D1. Long horizontal roof lines should be broken up by providing articulations in the façade of the building, changes in the height portions of the roofs, or change in color, material, forms, etc. Exceptions may be permitted only where a specific architectural style offers other types of roof forms and roof articulation.
- D2. Roofs shall be an integral part of the building design. Proposed parapets and roof screens shall be integrated into the roof design. The material and color of the roof screens shall appear identical to those in the roof or building.
- D3. Due to the proximity of the Paso Robles Airport, reflective materials, equipment or structures on or near the roof are prohibited.

MATERIAL AND COLOR

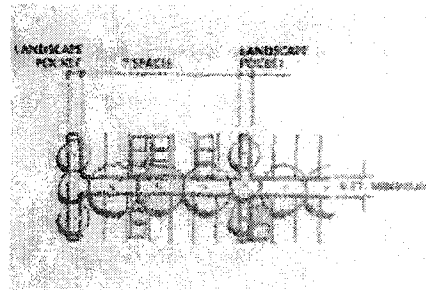
- E1. A material and color scheme has been developed for the project. Material and color variations in multi-building complexes shall be complementary and compatible among structures. Design shall be “hard edge” upscale business park with liberal use of concrete, stucco, glass and accent trims. Colors shall be kept in the neutral grays and beiges with colored accent areas.
- E2. No metal sided buildings of any type are allowed.
- E3. Large expanses of smooth material such as concrete shall be broken up with expansion joints, reveals, or changes in texture and color.
- E4. Large expanses of highly reflective surface and mirror glass exterior walls shall not be permitted.
- E5. Bright, contrasting colors shall be used for trim and accent areas only.
- E6. Material and colors for wall and ground signs shall be compatible with the buildings on the site.

III. PARKING AND CIRCULATION

- A1. To avoid large expanses of paved areas and to provide easy accessibility to buildings, large parking lots should be divided into smaller parking areas and dispersed around the site where possible.
- A2. Where large parking lots are needed, adequate landscaped pockets shall be integrated into the parking areas.
- A3. Pedestrian and automobile traffic paths shall be separated and conflict areas should be minimized.
- A4. Landscape buffers shall always be provided between parking lots and public streets, and parking areas and buildings.
- A5. Bicycle lockers and/or racks shall be located near building entrances.
- A6. Security kiosks and gates shall be located to allow queuing for at least 3 cars.
- A7. Whenever security gates are provided, sufficient parking should be provided outside the gate area for visitors.



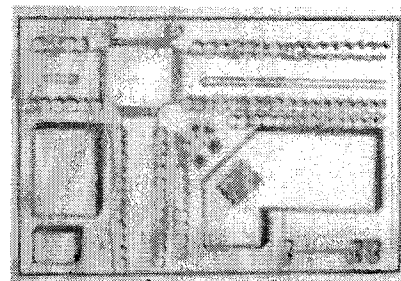
- A8. Shared driveway access between adjacent properties is encouraged.
- A9. Parking lots shall have adequate directional signs for visitors, delivery vehicles and employees.



IV. LANDSCAPING

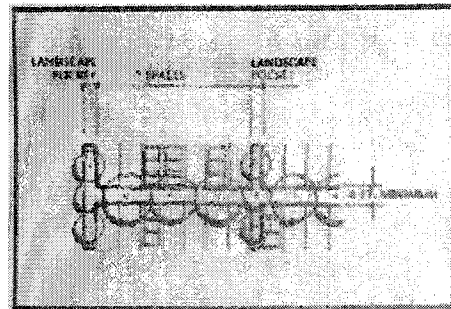
- A1. Landscaping design shall follow an overall concept and shall link various site components together.
- A2. All areas not in use by structures, driveways, and parking spaces shall be properly landscaped.
- A3. Landscaping shall consist of live plant materials. Rock and gravel may be incorporated into the landscaping to enhance the design but shall not replace live material.
- A4. Landscape materials shall adhere to the City's water wise standards. Drip irrigation shall be incorporated to the fullest extent possible.
- A5. Walls and fences shall be integrated into landscape design.
- A6. Any site furniture, fixtures and light fixtures must be incorporated into the design landscaping and follow the same design concept as the major structures on the site.
- A7. Future phases of a site already cleared and graded shall be adequately treated (hydro- seeded) to prevent erosion.
- A8. Sites adjacent to creeks and waterways, shall give special attention to landscape, fence, and wall design in order to enhance natural features and minimize development impacts.

- A9. A minimum ¹⁵ foot landscape strip shall be provided along all ~~public~~ street frontages in the project.

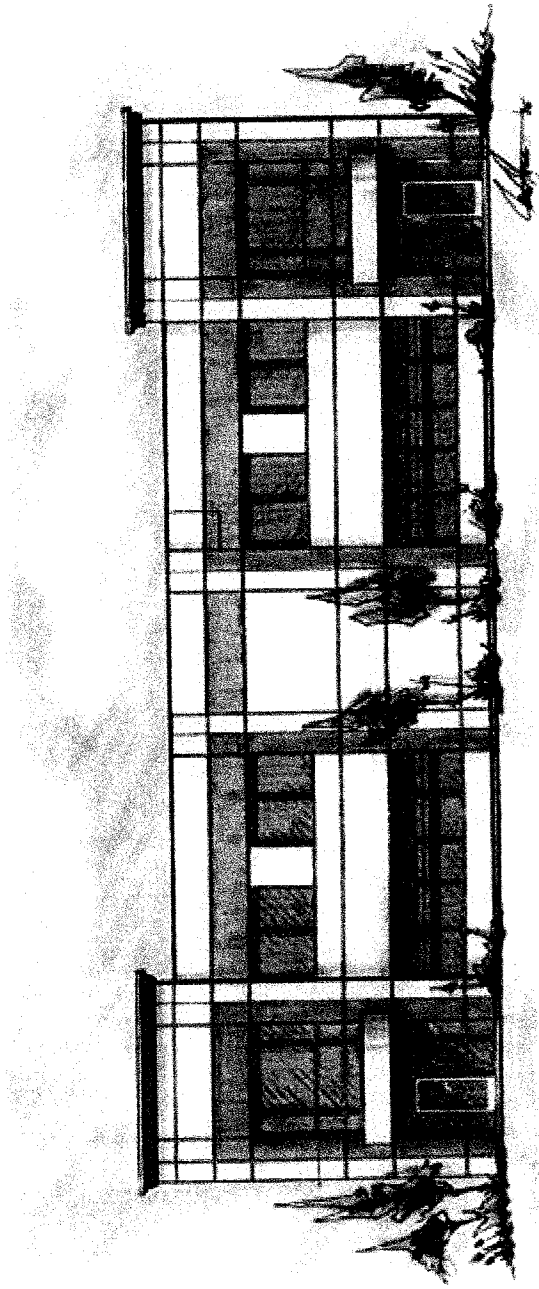


- A10. A minimum of 5 foot wide landscape strips shall be provided along the sides and rear property lines. *(Unless on a corner lot where 15-foot strips would be required on each street frontage).*

- A11. A combination of trees, shrubs and ground cover shall be used between the buildings and public right of way. Drought tolerant ground covers are highly encouraged in lieu of grass. Annuals and other non-draught tolerant plant material shall be used minimally and shall be used in high visibility areas.
- A12. Street trees shall be combined with on-site trees to create a double row of trees.
- A13. Liberal use of landscape berms shall be used whenever possible in the street frontage area of the site.
- A14. Landscape should be integrated with hardscape such as textured pavings, water features, trellises and walkways or art work to enhance the overall design of the site.
- A15. When parking lots are adjacent to public right of way, parking shall be screened by combining berms, shrubs and trees.
- A16. When landscape areas are provided between parking spaces facing each other, a minimum width of island shall be 6 feet. Landscape pockets shall be provided for every 7 parking spaces and at the end of each row of parking spaces.
- A17. When tree wells are provided, they shall be a minimum size of 5 feet square.
- A18. Buildings shall be separated from parking areas by landscaping and walkways.
- A19. All parking and landscape areas shall be separated by a 6 inch wide, poured in place, concrete curb.
- A20. Different color or textured material shall be used to define primary pedestrian access ways on site.
- A21. Lighting shall be of a type that is a "shielded" lighting type for both buildings and parking lot areas. Standard for the development is the KAD soft square lens "cutoff" light with a dark bronze finish and square pole by Lithonia Lighting (see appendix for details)












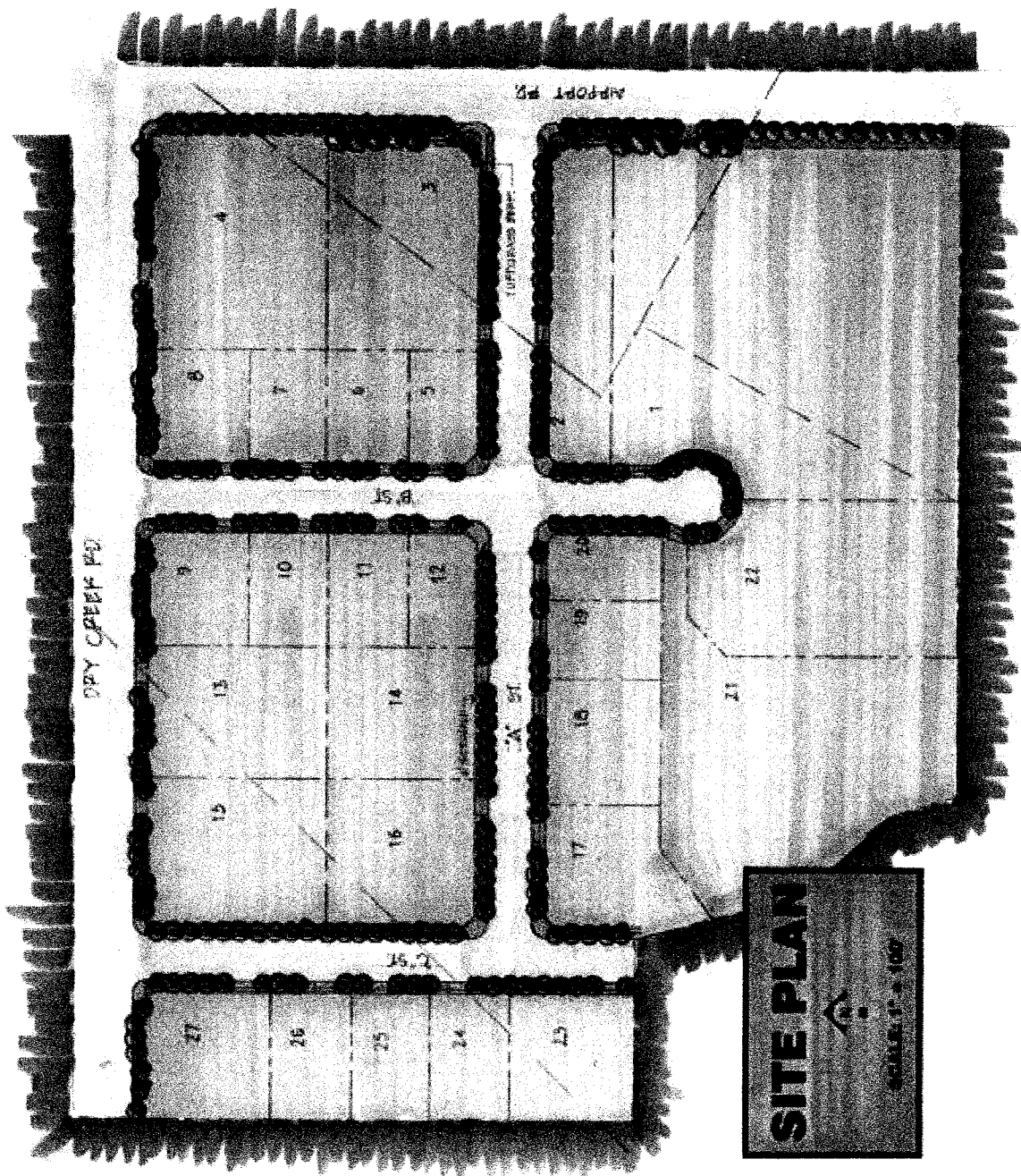
V. APPENDIX



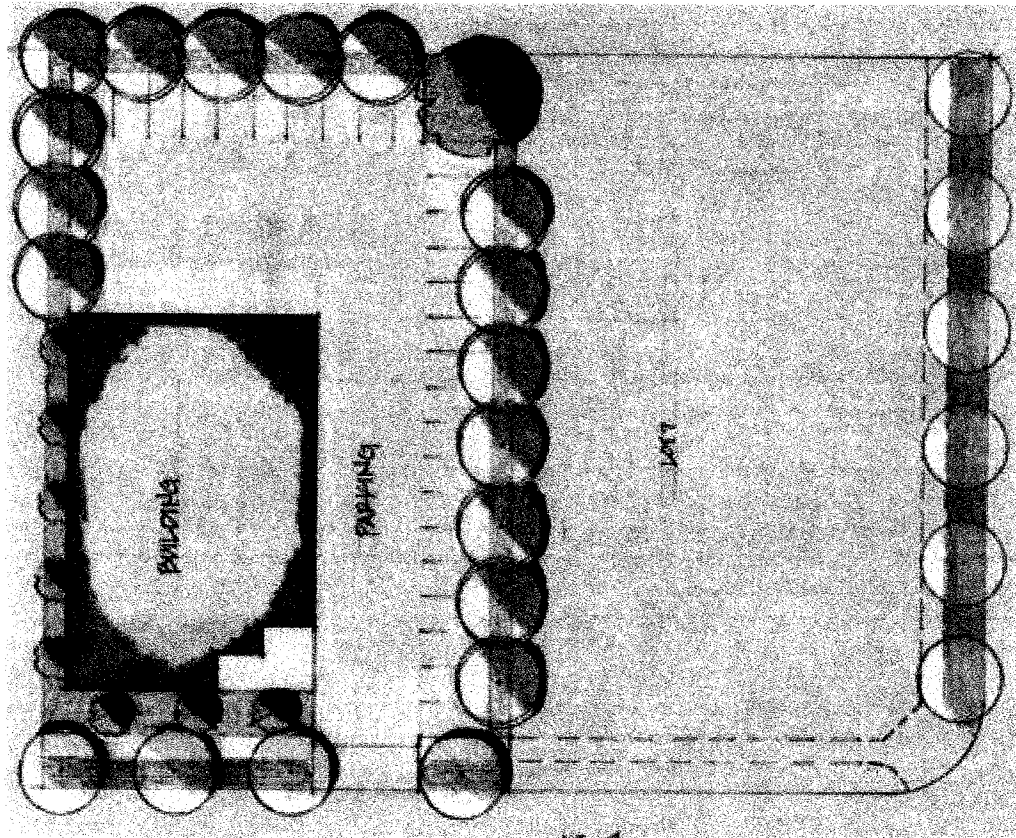
Typical Proposed Building Elevation

Color Schedule - Elevations

Trim	Accent	Main
		
		
		



Lot # 6 Detail



Plant List

Trees

- Botanical Name
- Arbutus menziesii
- Cedrus deodara
- Fraxinus angustifolia 'Raywood'
- Lagerstroemia indica
- Pistacia chinensis
- Platanus acerifolia 'Bloodgood'
- Pinus calleryana
- Quercus agrifolia
- Quercus lobata

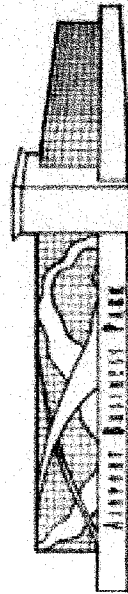
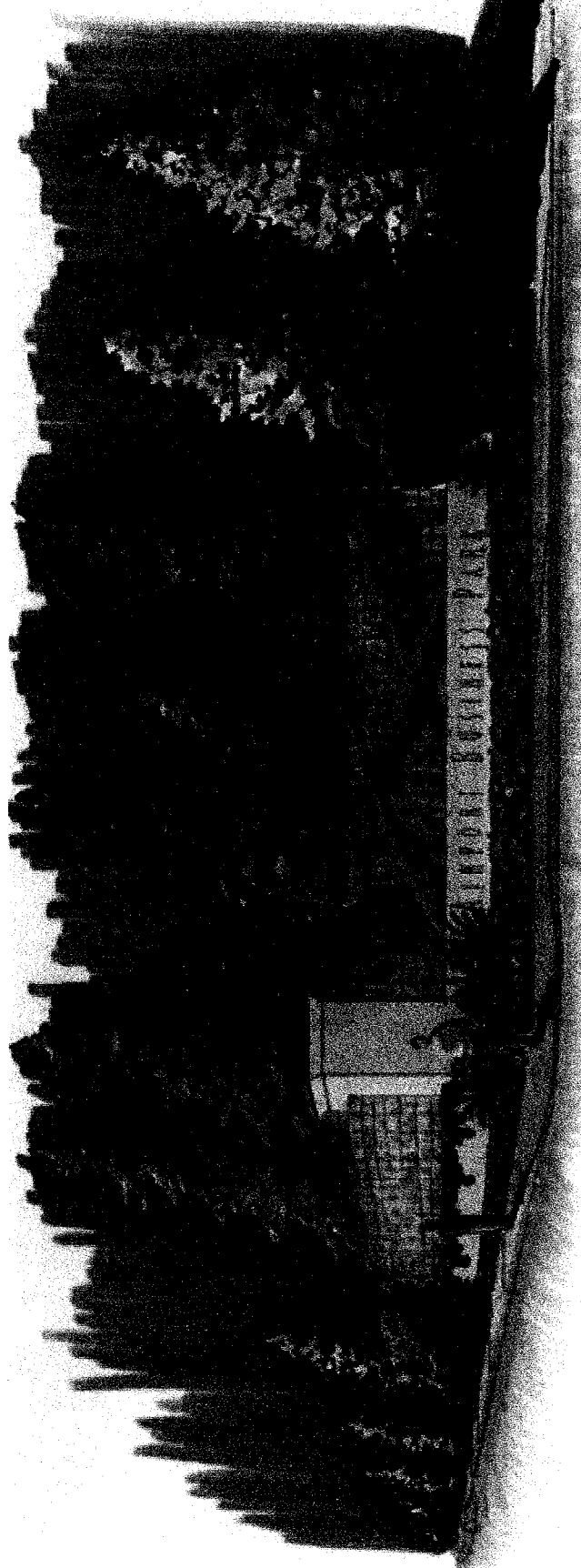
Shrubs

- Botanical Name
- Arctostaphylos densiflora 'Howard McMillen'
- Arctostaphylos emerald carpet
- Baccharis pilularis (twin peaks)
- Carpenteria californica
- Ceanothus concha
- Ceanothus griseus horizontalis 'Carmel Creeper'
- Hemerocallis
- Hesperomyles arbutifolia
- Heuchera
- Penstemon
- Rhamnus californica
- Ribes speciosum
- Rosmarinus officinalis 'Collingwood Ingram'

- Common Name
- Madroño
- Deodar Cedar
- Raywood Ash
- Crape Myrtle
- Chinese Pistache
- London Plane Tree
- Fruitless Pear
- Coast Live Oak
- White Oak

- Common Name
- McMillen's manzanita
- Emerald Carpet manzanita
- Dwarf Coyote Bush
- Bush Arнемone
- Wild lilac
- Carmel Creeper
- Day-lily
- Toyon
- Coral Bells
- Beardtongue
- California Coffeeberry
- Current
- Rosemary

Airport Road Entrance





HOME COMPANY PRODUCTS RESOURCES SALES INFO TRAINING LIGHT*LINK CONTACT SEARCH >

OUTDOOR LIGHTING

Expand All Folders

AREA LIGHTING

- Architectural
- Dusk-to-Dawn Security Area Lighting
- General Purpose
 - KAD: Contour Soft Square, Drop or Flat Lens Cutoff
 - KADT: Contour® Soft Square Suspend, Drop or Flat Lens Cutoff
 - KAC: Contour Soft Square, Drop or Flat Lens Cutoff
 - KC: Contour Die Cast Cutoff with Pole
 - KT: Low Pressure Sodium Cutoff
 - KVS: Square Cutoff
 - KAR: Centriform Cutoff
 - KQS: Open Square Post-Top Cutoff

Security

BUILDING-MOUNTED

- Architectural Wall Packs
- Cast Micro Wall Packs, Polycarbonate Refractor
- Cast Wall Packs, Polycarbonate Refractor
- Cutoff Mini Wall Packs, Polycarbonate Refractor - Contour Series
- Decorative Wall Packs
- Die-Cast Wall Packs, Glass Refractor
- Mini Wall Packs, Polycarbonate Refractor - Contour Series
- Polycarbonate Wall Packs

FLOODLIGHTING

- Architectural
- General Purpose
- Hazardous
- High-Performance
- Large Floodlight - Contour Series
- Medium Floodlight - Contour Series
- Micro Floodlight
- Mini Floodlight - Contour Series
- Motion Sensors
- Quartz Floodlight

PARKING GARAGE-CANOPY

- Canopy
- Parking Garage

KAD: CONTOUR SOFT SQUARE, ~~DROP~~ OR FLAT LENS CUTOFF *(No exposed lens)*

Product Catalog > Lithonia Lighting > Outdoor Lighting > Area Lighting > KAD

Intended Use: For parking areas, street lighting, walkways and car lots.

Features: Housing – Rugged, die-cast, soft-corner aluminum housing with 0.12" nominal wall thickness. Extruded 4" soft corner arm for pole or wall mounting is standard.

Door Frame – KAD die-cast door frame has impact-resistant, tempered glass lens which is fully gasketed with one-piece bonded tubular silicone. KAC die-cast aluminum door frame has prismatic, impact-resistant, tempered glass, drop dish acrylic lens or drop dish polycarbonate lens. Door frame is fully gasketed with one-piece tubular silicone.

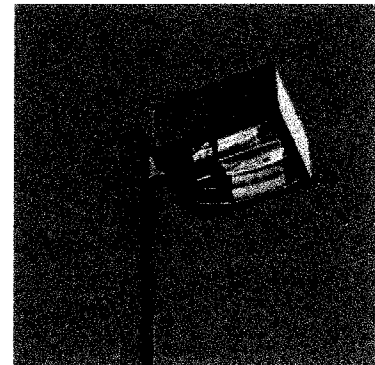
Optics – KAD reflectors are anodized hydroformed and segmented aluminum. Four cutoff distributions available: R2 (Roadway), R3 (Asymmetric), R4 (Forward Throw) and R5 (Symmetric). Segmented reflectors are rotatable and field-interchangeable. KAC reflector is optical-quality aluminum that works in tandem with a light-diffusing prismatic lens.

Electrical – Ballast is high-reactance, high power factor (70-150W HPS, 100M & 150M) or high power factor constant-wattage autotransformer (175-400W MH & HPS). Ballast is copper wound and 100% factory tested.

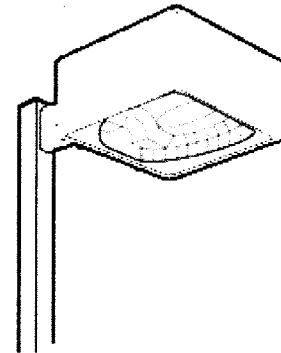
Finish – Dark bronze (DDB) corrosion-resistant polyester powder finish (DDB), with other architectural colors available.

Socket – Porcelain, horizontally (position) oriented mogul-base socket (100M & 150M are medium-base) with copper alloy, nickel-plated screw shell and center contact. UL listed 1500W-600V. 4KV pulse rated.

Listings: Listings – UL Listed (standard). CSA Certified or NOM Certified (with option suffix). UL listed for wet locations. IP65 certified.



Additional Images



Product Catalog Page

Specification Sheets

Photometric Data

Questions about this product?

NOTE: pole height should be no taller than 12-feet high.

PROOF OF PUBLICATION

LEGAL NEWSPAPER NOTICES

PLANNING COMMISSION/CITY COUNCIL
PROJECT NOTICING

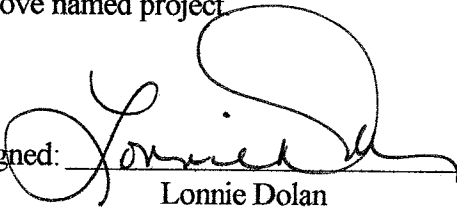
Newspaper: Tribune

Date of Publication: September 20, 2006

Meeting Date: October 10, 2006
(Planning Commission)

Project: Planned Development 05-016
and Tentative Tract 2772
(Mondo-s/w corner Airport &
Dry Creek Roads)

I, Lonnie Dolan, employee of the Community
Development Department, Planning Division, of the City
of El Paso de Robles, do hereby certify that this notice is
a true copy of a published legal newspaper notice for the
above named project

Signed: 
Lonnie Dolan

forms/newsaffi.691

CITY OF EL PASO DE ROBLES

NOTICE OF PUBLIC HEARING;

NOTICE OF INTENT TO ADOPT A
MITIGATED NEGATIVE DECLARATION

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of El Paso de Robles will hold a Public Hearing to consider adoption of a Mitigated Negative Declaration (statement that there will be no significant environmental effects because of the required mitigation measures) in accordance with the provisions of the California Environmental Quality Act (CEQA), and approval of a Planned Development for the following project:

Planned Development 05-016 & Tract 2772: Development Plan and Tentative Tract applications filed by John McCarthy on behalf of Doug Mondo, to construct a 27-lot business park on the property located on the southwest corner of Airport Road and Dry Creek Road.

The public review period for the Draft Mitigated Negative Declaration commences on September 20, 2006, and ends at the Public Hearing, which is scheduled for the Planning Commission on Tuesday, October 10, 2006.

The meeting will begin at the hour of 7:30 pm in the Conference Center (First Floor) at the Paso Robles Library/City Hall, 1000 Spring Street, Paso Robles, California. All interested parties may appear and be heard at this hearing.

The proposed Mitigated Negative Declaration may be reviewed at the Community Development Department, 1000 Spring Street, Paso Robles, California. Copies may be purchased for the cost of reproduction.

Written comments on the proposed development plan, tract and mitigated negative declaration may be mailed to the Community Development Department, 1000 Spring Street, Paso Robles, CA 93446 provided that such comments are received prior to the time of the public hearing. Oral comments may be made at the hearing. Should you have any questions regarding this application, please call Darren Nash at (805) 237-3970.

If you challenge the development plan, tract or mitigated negative declaration in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning Commission at, or prior to, the public hearing.

Darren Nash, Associate Planner
September 20, 2006

6455586

PROOF OF PUBLICATION

LEGAL NEWSPAPER NOTICES

PLANNING COMMISSION/CITY COUNCIL
PROJECT NOTICING

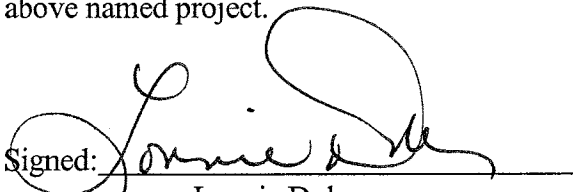
Newspaper: Tribune

Date of Publication: September 29, 2006

Meeting Date: October 10, 2006
(Planning Commission)

Project: Planned Development 05-016 &
Tract 2772 (Mondo - Airport &
Dry Creek Roads)

I, Lonnie Dolan, employee of the Community
Development Department, Planning Division, of the City
of El Paso de Robles, do hereby certify that this notice is
a true copy of a published legal newspaper notice for the
above named project.

Signed: 
Lonnie Dolan

forms\newsaffi.691

CITY OF EL PASO DE ROBLES
NOTICE OF PUBLIC HEARING:

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of El Paso de Robles will hold a Public Hearing to consider approval of the following project:

Planned Development 05-016 & Tract 2772: Development Plan and Tentative Tract applications filed by John McCarthy on behalf of Doug Mondo, to construct 36 lot business park on the property located on the southwest corner of Airport Road and Dry Creek Road.

The meeting will begin at the hour of 7:30 pm in the Conference Center (First Floor) at the Paso Robles Library/City Hall, 1000 Spring Street, Paso Robles, California. All interested parties may appear and be heard at this hearing.

Written comments on the proposed development plan and tract may be mailed to the Community Development Department, 1000 Spring Street, Paso Robles, CA 93446 provided that such comments are received prior to the time of the public hearing. Oral comments may be made at the hearing. Should you have any questions regarding this application, please call Darren Nash at (805) 237-3970.

If you challenge the development plan or tract in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning Commission at, or prior to, the public hearing.

Darren Nash, Associate Planner
September 29, 2006 6462763

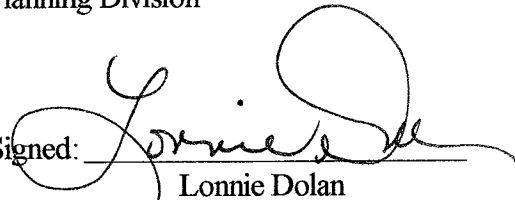
**AFFIDAVIT
OF MAIL NOTICES**

PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING

I, Lonnie Dolan, employee of the City of El Paso de Robles, California, do hereby certify that the mail notices have been processed as required for Planned Development 05-016 and Tract 2772 (Mondo – Airport & Drycreek Roads) on this 26th day of September, 2006.

City of El Paso de Robles
Community Development Department
Planning Division

Signed: _____



Lonnie Dolan

forms\mailaffi.691